

Australian Model Railway Association

# JOURNAL

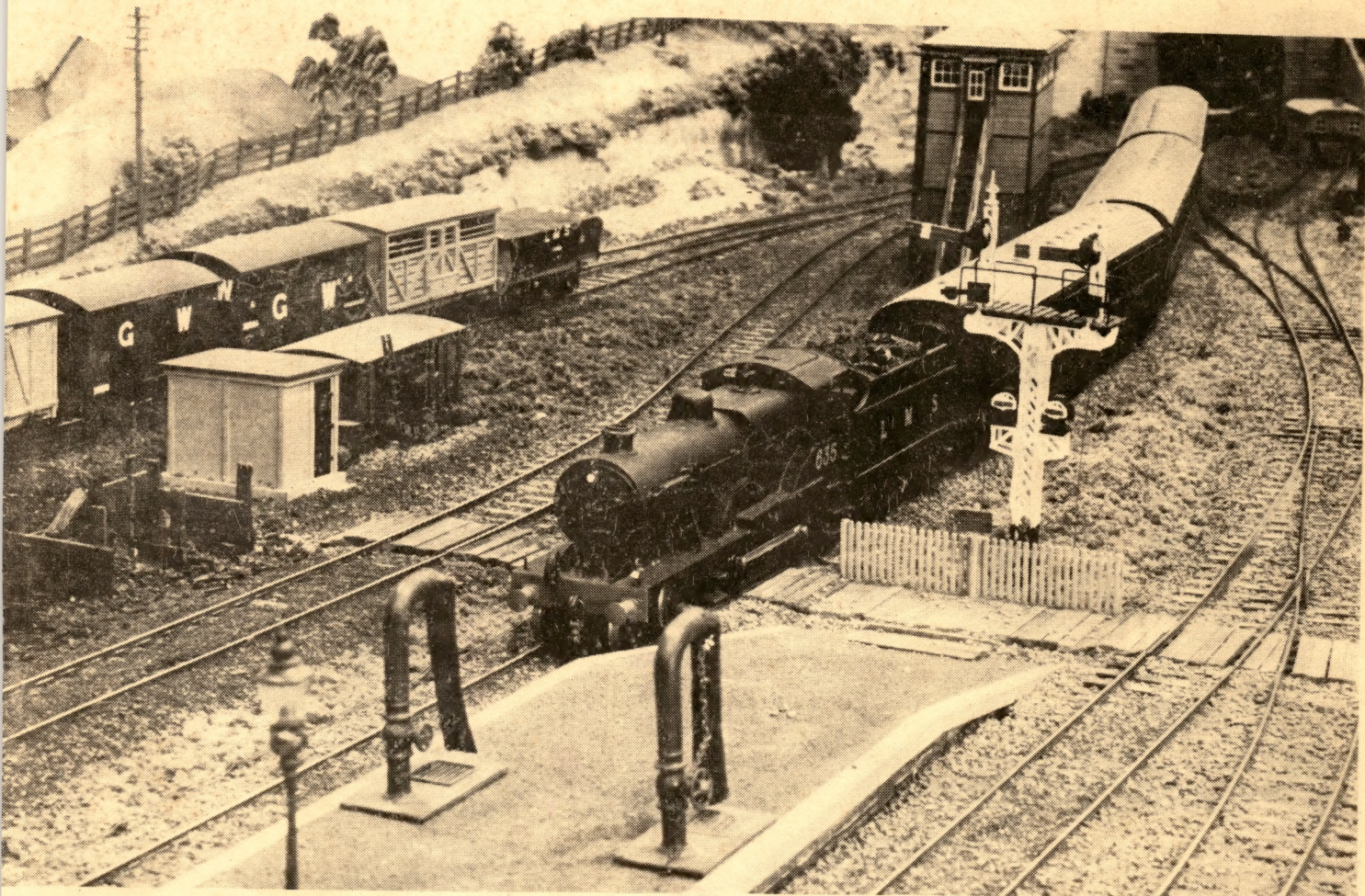
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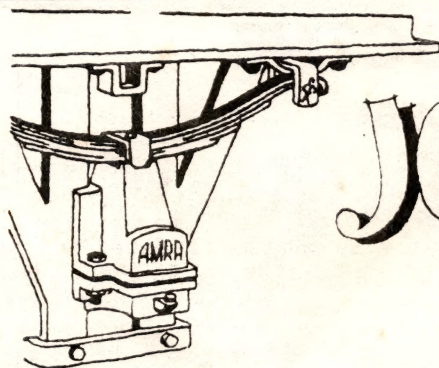
5 CARRINGTON ROAD

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MODEL RAILWAY EQUIPMENT FOR ALL AGES







# Editorial JOURNAL BOX

I have just had the opportunity to observe and compare the railways of two states over a period of two weeks, and I find that they are like chalk and cheese. The only similarity seems to be the lack of a guard's van on both systems.

Perhaps that is one of the reasons that we all still model railways. The endless variety that is available - from rolling stock to scenery. You do not have to look far on any system to be able to pick up ideas to help your modelling.

Having had a dearth of articles in the last two Journals, things have happened and we have a few for this one, although a couple may be kept until the next issue to make room for the Membership lists. We still have to watch the overall weight of Journal for the Bulk Posting.

With the Silly Season over by now, resolutions made and, perhaps, broken by now, let me wish you all happy modelling in 1987.

Rex Little  
Editor

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We seem to have had a dearth of construction or modelling articles lately. The last two issues have consisted mainly of Branch Notes. I have tried to redress that balance with my trestle article.

For those who are interested in mini computers, a recent development has been 'desk-top publishing' using a laser printer to produce art work of quality close to typeset quality.

The cost of such systems is decreasing and it would seem to be the way to go in the future to greatly improve the presentation of Journal. It's a pity the capital cost is currently beyond our resources.

Roger Lloyd  
Managing Editor

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## On The Cover

A scene on Phil Knife's EM scale Somersetshire Midland Railway displayed in the 1986 NSW Branch Exhibition. More photos in this issue.

Photo by Jack Parker

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# THE FEDERAL SCENE

## THE SECRETARY'S DESK

Or We Haf Ways of Makink You Read!

Well, we could if this were indeed 'Cowards Castle', but as it isn't, we are still back where we started when first I put pen to paper under this heading. Once again I can but ask that each of you READ what is written before making comment or taking action. Please READ the following once again.

All general matters, communications, complaints, etc, directed to the Federal COM are to be mailed to the Federal Secretary. All matters pertaining to Membership, Renewals, change of address, etc, are to be mailed direct to the Federal Registrar. The Federal Registrar supplies the Membership lists for the mailing of Journal. If you don't want to miss out on your Journal, let the Registrar know when you change address, etc.

Having got that over with, may I, on behalf of the Federal COM, wish each and every one of you a HAPPY NEW YEAR.

The Federal Committee of Management for 1987 is, surprise, surprise, much the same as that of 1986, i.e.

President  
Vice President  
Treasurer  
Registrar  
Secretary  
Committeeman/Pro

Keith Wilcox  
Brian Tyson  
Ken Edwards  
Norm Read  
Phil Kelly  
Col Steele

\*\*\*\*\*

On behalf of the COM, many thanks to retiring member, Bob Wardrop for his services and input to the Federal COM. On behalf of the COM also, many thanks to another retiring Federal worker, our co-ordinator of "Member's File", Bruce Norton, who is moving once more from East to West.

WANTED!!!! One or even two volunteers to take over as Co-ordinator(s) of "Member's File". Geographical location is of no significance, as most of the work involved can be communicated by mail.

During this year, the COM intends to put forward a draft proposal for changes to the Federal Constitution. Once the draft has been distributed via Journal, there will be a SHORT period for objections, suggestions, etc, to be made to Federal COM before the draft or revised draft is put to a ballot of members.

Just in case anyone has missed this column in the last few issues of Journal, I have been indulging myself a little in that wonderous hobby of railway modelling. Might even get to put pen to paper on that topic sometime.

In closing, let me paraphrase a saying of the late US President John F Kennedy --- 'Ask not what AMRA can do for YOU, ask what YOU can do for AMRA.' In so doing, you may be surprised at just what benefits you do gain for yourself.

Regards to all.

Phil Kelly

## PRESIDENT'S ANNUAL REPORT : 1986

In presenting this report to the 35th Annual General Meeting of the Federal Committee, it is pleasing to note that the previous 12 months to the end of August were reasonably successful, with a pleasing increase in membership and strengthening in our financial position.

Our current membership is 723, an increase of 71, which is heartening to all concerned, and hopefully this trend will continue, despite some cost increases, which, fortunately, were not gigantic; the joining and membership fees have remained static, and unless there are any massive increases, there should not be any change in the foreseeable future.

The Federal COM recently investigated the necessity of incorporation and have been informed that, whilst it is necessary for the State Branches, as we do not own any real estate or hold public functions, but, rather a co-ordinating body, we are not governed by this requirement.

Currently, certain clauses in the Federal Constitution, including those covering membership and elections, are being looked at and members will be requested to vote on recommended amendments

in the new year, which has always been the practice.

Regretably, criticism has been levelled at the editorial committee over content or lack of same in Journal, which is not warranted, as they can only publish articles, etc, if and when received. Once again, 'Vic Print' deserve commendation for their efforts, as do all involved in the preparation of Journal.

One matter of concern to the COM is the lack of progress being made with the Member's Information File, which, in part, is due to a lack of input. We realise that a certain amount of research is required, and it may be necessary to try and form a sub-committee to take over this task rather than leaving it to one person.

I would like to take this opportunity of thanking the retiring Federal COM for their support and diligence applied to their allotted tasks.

In conclusion, due to problems with my health, this will be my last year in office and would like to express the hope that this 12 months will be a favourable one for the Association and all members.

Keith Wilcox

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### A.M.R.A.

### HON. LIFE MEMBERS

#### UP TO 1987

Rick Richardson 1955  
Tim Dunlop 1956  
Mayer Levy 1967  
Alan Wilson 1961  
Arthur Harrold 1961  
Margaret Dunlop 1972  
Jack Treseder 1965  
Cedric Rolfe 1970

Faith Dean 1972  
Ernie Dean 1972  
Norm Read 1973  
Rex Little 1974  
Maurie McKinnon 1976  
June Dunn 1978  
Stuart Westerman 1980  
Bob Gorrell 1981  
Mal Baker 1983



AUSTRALIAN MODEL RAILWAY ASSOCIATIONFEDERAL COMMITTEE OF MANAGEMENTSTATEMENT OF RECEIPTS AND EXPENDITUREFOR THE YEAR ENDED 30 JUNE 1986RECEIPTSFunds at 1 July 1985

Cash at bank	2,173.35	
Petty Cash on hand	100.00	
Building Society	2,659.89	
		<u>4,933.24</u>
Joining fees		1,104.00
Donations		57.15
Journal advertising		824.13
Interest received		281.33
Sale of guides		142.00
Membership renewals		5,565.00
Competition trophies		131.60
New membership subscriptions		1,186.00
Sale of journals		5.00
		<u>\$14,229.45</u>

EXPENDITURE

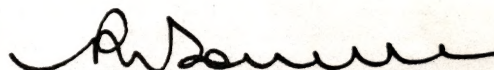
Printing of Journal	4,279.78
Postage of Journal	1,055.33
Journal expenses	454.54
Stationery	961.22
General postage	643.21
Trophies	93.30
Bank charges	6.00
FID and FDT	15.06
Badges	409.50
Duty stamps	27.50
Shields and engraving	179.40
General expenses	32.68
Photocopies	96.00
Insurance	175.88

Funds at 30 June 1986

Cheque account	2318.38
Advertising account	324.07
Publishing account	247.33
Investment account	<u>2842.95</u>
	5732.73
Petty Cash on hand	67.32
	<u>5,800.05</u>
	<u>\$14,229.45</u>

AUDITOR'S REPORT

I have examined the books and records of the Federal Committee of the Australian Model Railway Association and report that, in my opinion, the above statement is a true and fair record of receipts and expenditure during the year ended 30 June 1986 and of cash funds on hand at that date.

  
R W GORRELL - F C A

Dated this 19 day of August 1986.

\*\*\*\*\*

A.M.R.A.MERITORIOUS AWARDSUP TO 1987

Bob Gorrell	1965	Eric Doherty	1975	Simon Mead	1978	John Hill	1983
Ivor Bunker	1967	June Larmour	1975	John Harry	1979	Corinne Bunker	1983
Alan Dowel	1969	Fyfe Thorpe	1976	Harold Warren	1979	John Martin	1983
Stephen Suggit	1969	Eric Lyon	1976	Cec Wall	1979	Keith Wilcox	1983
Rex Little	1970	John Skilton	1976	Jack Eagles	1979	Norm Chapple	1984
Norm Read	1970	Keith Robinson	1976	Ted Thoday	1980	Roger Lloyd	1984
Mal Baker	1971	Dot Treseder	1977	Audrey Cornish	1980	Val Hogan	1984
Glyn Shepherd	1971	Tony Gray	1977	Ray Brownbill	1980	Bob Mawson	1984
John Sneddon	1972	Jim Christie	1977	Bob Edwards	1981	Manfred Ebinger	1985
John Dunn	1972	Jack Parker	1977	Graham Watson	1981	David Bennett	1985
Graham Larmour	1972	Rup Ackland	1977	Steve Malone	1982	Gus Durham	1985
Ken Down	1973	Bill Moorehouse	1978	Alan Porter	1982	Fred Green	1986
David Ellis	1973	George Bray	1978	Sturat Westerman	1982	Bill Secker	1986
Arthur Robinson	1974	Arthur Hayes	1978	Bob Wardrop	1982	Stuart Wall	1986
Bruce Lovett	1974			Gordon Duncan	1983		



# TO CONSTRUCT A TRESTLE

by Roger Lloyd

All photos by the author

Probably my first inclinations to construct a trestle started stirring when I visited Rick Richardson's Vulcan Vale Railway. In the rebuilding of the Victorian Branch layout, I planned a trestle to go in a prominent place in the centre promontory facing the entrance door.

Reading Rick's excellent discourse on the trials and tribulations of trestle building in AMRM (Issue 135, December 1985) finally galvanised me into action. During my holidays in February I constructed the curved trestle for our mainline, depicted in the accompanying photograph.

After many hours' work, I swore never again, but I'm afraid I am unable to say no to either pretty girls or those plotting types at the Club who suggested we need another trestle, this time for the HOn3 line.

As the plaster started covering over scenery, our arm twisters skilfully left a gaping hole where the new trestle was to go! As this offended my sensitivities, all other projects were dropped and off to work.

Having learnt by my mistakes on the first trestle, this one took only about 20-24 hours to finish to installation stage. Incidentally, the first trestle was made mainly from basswood and balsa, but for this one, it is all balsa, except for the decking of the refuge.



Two views of the first trestle in the AMRA Victorian Branch layout with a Trax 830 class trundling across. Note on taller trestles, the additional braces between the piers.





Why balsa? Well, it's cheap, easy to work and is quite strong enough. Its other advantage is that it stains beautifully to the typical leached out gray appearance of old exposed timber.

Well, that's enough of the preliminaries, now to the construction; although this is a Victorian prototype trestle, I'm sure the principles will apply to all Australian prototypes.

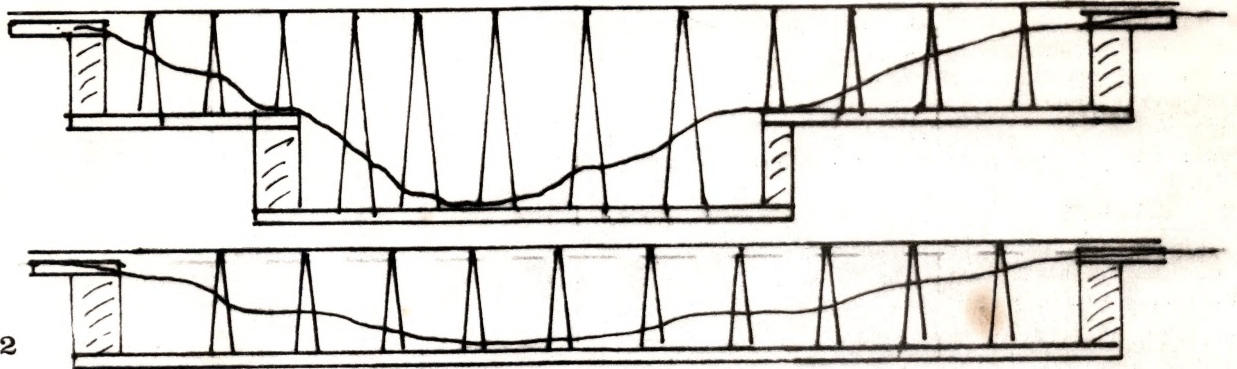
#### BILL OF MATERIALS

$\frac{1}{4}$ " x  $\frac{1}{4}$ " balsa strips - for piles  
 1/16" balsa sheet  
 3/32" balsa sheet  
 Gap filling ACC glue, such as Network 2000  
 PVA wood glue  
 Timber or particle board for the base  
 Sharp hobby knife  
 Patience  
 Understanding spouse

#### 1 THE BASE

Depending on how deep you wish to make the trestle, you can have either a three-piece base as per Figure 1 or a one piece base as per Figure 2. I used particle board for the base as I was building the trestle at home to be placed on site on completion.

I would not recommend building in situ.



#### 2 PREPARING THE PILES

Four are required for each bent. In Rick's article he describes in some detail characteristics of Australian Trestles compared with USA prototypes. Our's make use of the long eucalyptus trunk to form the piles, whereas USA trestles used sawn timber. To turn my  $\frac{1}{4}$ " square balsa strips into rough round piles, I used a sanding disk on my electric drill mounted in a drill stand.

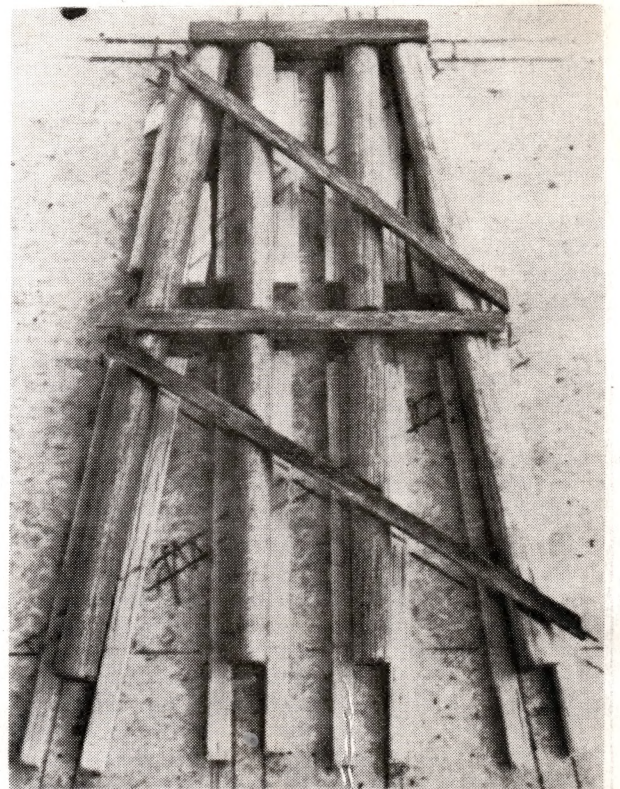
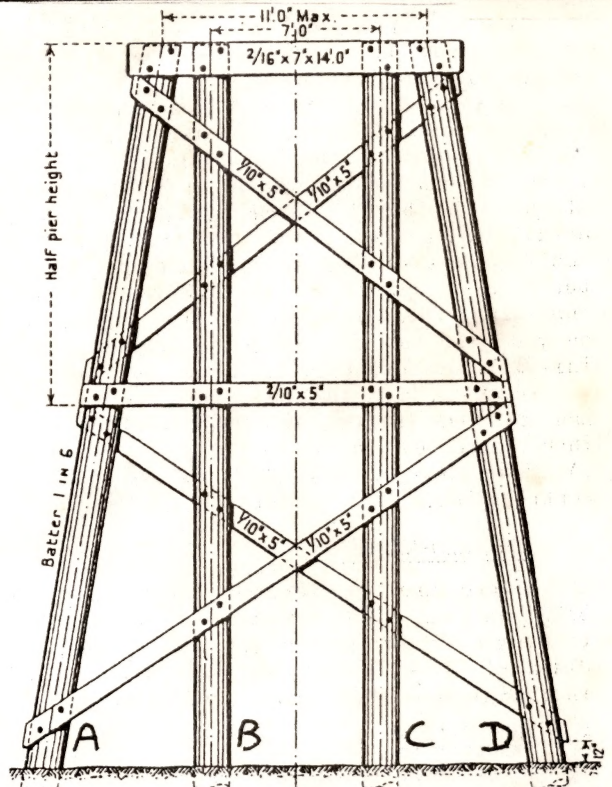
But be careful! A sanding disk will strip away balsa very quickly so easy does it. By the way, this method generates copious quantities of fine balsa dust, so I suggest you wear goggles and a face mask.

Lightly sand each of the four corners of the strip until you have an eight sided strip. Now applying very light pressure against the disk, spin the strip around between your fingers to round off the strip. Hand finish if necessary to smooth off any major irregularities, but don't be too perfect - natural trees aren't.

Cut the piles to length - the centre piles are just a little shorter than the outside piles, but don't be too concerned at this stage, they can be trimmed when the bent is constructed later.



A pile notched and ready for assembly.



A bent under construction in the jig. When set, the bent is removed and the diagonal braces on the other side are affixed.



Now for a tedious task - notching the piles for the crossheads and braces. Crossheads are a scale 6" apart, so in HO, leave 1.75 mm of material and cut down 4 mm. For the braces, only a shallow notch of about 1 mm is needed.

To assist in the notching and in the later gluing of the bents, construct a jig on an offcut of particle board. Draw a plan of the bent to actual scale size (refer to the photo). Glue small pieces of wood to hold the piles in place, but leave a gap where the horizontal braces go. Then draw the position of the diagonal braces on your jig, but only draw the braces on one side. This will avoid cutting incorrect notches.

Place a pile in the right hand position (D) and cut the notches on the front of the pile. Then place the pile in the left hand position (A) face down and notch. Do the same for the centre piles. Please refer to the diagram.

### 3 THE CROSSHEADS

There are two for each bent to be cut from 3/32" balsa. Cut strips 4.6 mm wide and cut to length 49 mm for broad gauge. The crosshead width of 7" scales out at 2.04 mm. 3/32" balsa is 2.38 mm wide, but that is near enough for me.

### 4 THE BRACES

I believe scale 10" x 5" scale lumber is made by North Eastern, but I used 1/16" balsa cut into strips a scale 10" deep. The braces are 5" wide which scales at 1.46 mm. 1/16" balsa is 1.59 mm thick, again near enough. Cut horizontal and diagonal braces to length - use your jig as a guide. If they are cut a little over length, it is easy later to trim to the correct length.

### 5 THE BEAMS

For 15' spans, the beam sizes are 18" x 7½". Cut 5.25 mm strips from 3/32" balsa. Four beams are required for each span.

### 6 THE STAINING OF THE TIMBER

This trestle was 30" long and has over 650 pieces - just imagine painting all those by hand! The mind boggles. But fortunately, there is an easy way and all those pieces can be painted in just a few minutes.

In a screw top jar, pour in about an inch of water. Add a few drops of drawing ink, a small amount of white artists acrylic paint and a few drops of detergent. Stir thoroughly and test on an offcut of balsa. It is much darker wet so leave the offcut to dry. If it is too light, add more ink. If too dark, more water and white paint.

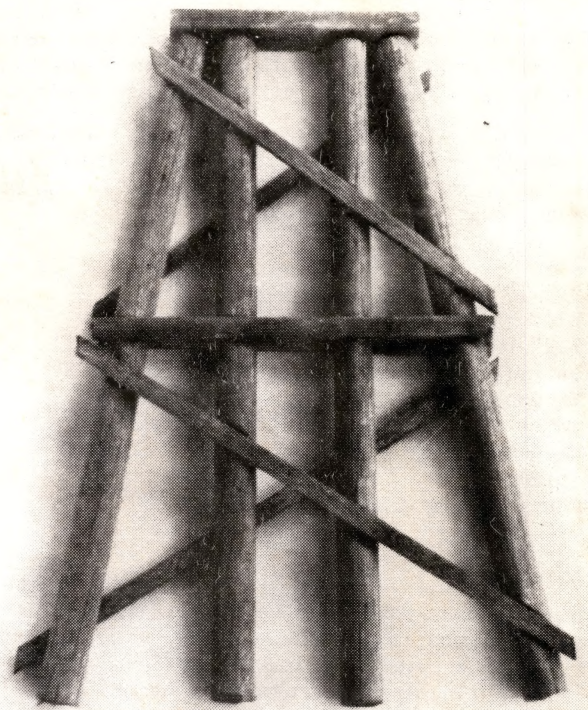
Once the mixture is right, drop your bents into the jar, screw on the lid, shake the jar and then decant the liquid into another jar. Tip the bents onto a sheet of newspaper and leave to dry. Repeat the process with the crossheads, beams and braces.

Retain the liquid as you have the decking timbers yet to stain.

### 7 THE BENTS OR PIERS

In your jig, place a crosshead and a horizontal brace in position. Superglue the piles to the crosshead and brace. Then glue the upper crosshead and horizontal and diagonal braces, ensuring that they sit correctly in the notches. The pieces will need holding for a minute or two as gap filling Superglue is slower setting than normal superglues.

When set, prise up the bent from the jig, turn it upside down and glue the diagonal braces on the other side.



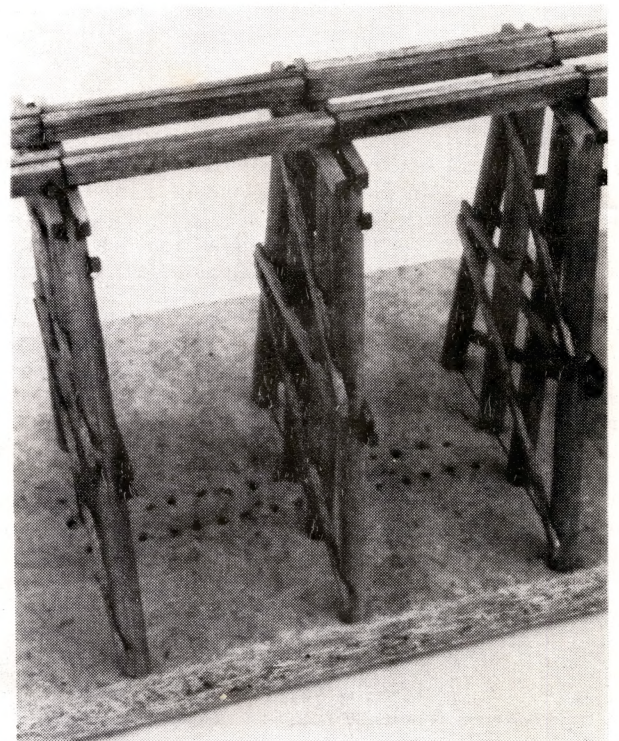
The completed bent

If the braces are a little long, trim off the excess length. Using a paint brush, dab a little stain on the unstained cut end. You will notice that the end grain readily absorbs the stain.

Trim the bottom of the piles so that the piles sit square and all the piers are the same height.

### 8 CONSTRUCTION STARTS

Using PVA glue, set the first two bents to the base; a span length apart. Use a square to ensure they are vertical. Superglue four beams between the two bents. Refer to the plans for distances between the beams. Note - if you are making a curved trestle, the outside beams are a little longer than the inside beams.



The piers are glued to the base with PVA, while the beams are set with superglue.



Allow the first two bents to completely set and then proceed to glue the next bent and beams. It won't take long to finish this task once you get going.

When completely dry, sand the tops of the beams to ensure they are level. Don't worry about restraining the tops of the beams as they are covered by the decking.

#### 9 THE DECK

Again I used 1/16" balsa. This is overscale for the 4" thick planks, but looks near enough. Cut across the grain a sheet the length of the deck timbers; for broad gauge 10'6" (scale 36.75 mm). Then cut with the grain strips 1.75 mm wide. When you have sufficient cut, stain the balsa by the previous method. For each refuge or safety bracket, cut three decking timbers an extra scale 42" long (longer on curved trestles).

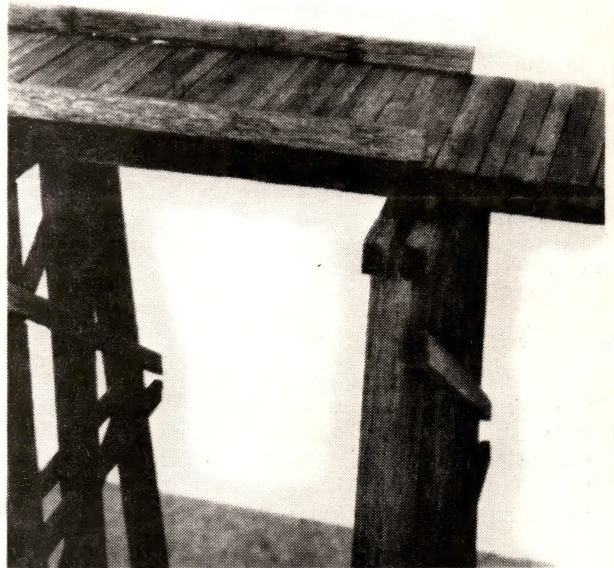
When dry, spread some superglue on the beams about 50 mm long and place some planks on the glued beams. It helps to have previously lined some planks on your work area where it is easy to pick them up between your forefinger and thumb. Use a straight object to align the planks. With gap filling superglue, you have a minute or so to make adjustments. Also, have a small flat object to place on the decking. Holding this object down lightly, push back the freshly glued timbers against the previously glued timbers to close the gaps.

Be careful to set the long timbers for the safety brackets in the right position - one centred over a bent, the other two a scale 2'6" apart either side.

When complete, the many variations in tone of the stained balsa decking timbers has a most satisfying appearance. It seems a pity to cover it with ballast!

#### 10 THE KERBS

These are 10" x 5" so 1/16" balsa as for the braces on the bents do here. For a straight trestle, cut to the same length as the beams;



The kerbs are superglued in place.

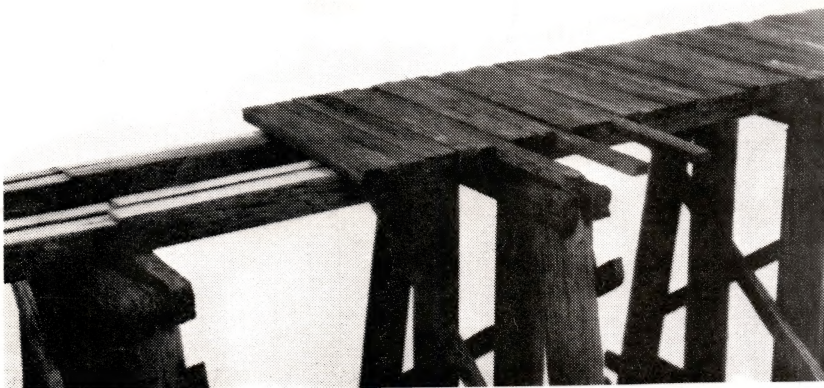
a scale 15' being the most common size. For a curved trestle, the outside kerbs are longer than the inside kerbs. Here, cut a little over length and trim as you go.

Stain the kerbs and simply superglue along the outside edges of the decking timbers. For any kerbs that need trimming, touch up the cut end with a dab of stain.

#### 11 THE REFUGE

When laying the deck, you have set the three longer deck timbers in position. Be careful here as the balsa will break easily until supported. A 6" x 4" supporting strut is glued to the centre deck timber down to between the crossheads.

Glue some 1/32" basswood or similar material across the deck beams to represent the 6" x 2" deck. Superglue some small strips around the upper edge of the deck.



After sanding the top of the beams, the deck is glued with superglue. Note here the three decking timbers extended for the refuge.

The refuge is formed with wire and glued into fine holes drilled into the timber.





Bend some fine wire to shape (refer to plan), drill small holes in the refuge and push the wire into the holes with a dab of superglue. Solder another wire between the two end wire brackets. Trim off excess wire with a cutoff wheel in a motor tool.

On a curved trestle, increase the distance of the safety bracket from the track to allow for the greater overhang of the model. I made this mistake on my first trestle and the bracket scraped the sides of the longer cars.

Paint the wire an off white.

## 12 THE ABUTMENTS

Again, refer to the plans. Behind the first short piles are 18' long horizontal planks approximately 10" x 4" supported by three 12" x 6" sheeting posts. On the trestle at Eltham, there are more planks parallel to the track extending back from the abutment to support the ballast.

## 13 SUPERDETAILING

For that competition winning entry, extra details can be added, such as bolt heads, the 3" angle brackets, the 4" x 3" runner under the deck along the beams and some rust stains from the bolts and brackets.

## 14 INSTALLATION

With some carpentry, make up some supports for your trestle and glue and/or screw in position. Lay your track and glue the track down before applying ballast by your favourite method.

If handlaying track, glue the sleepers directly to the decking. Add a check rail on the inside curve of a curved trestle.

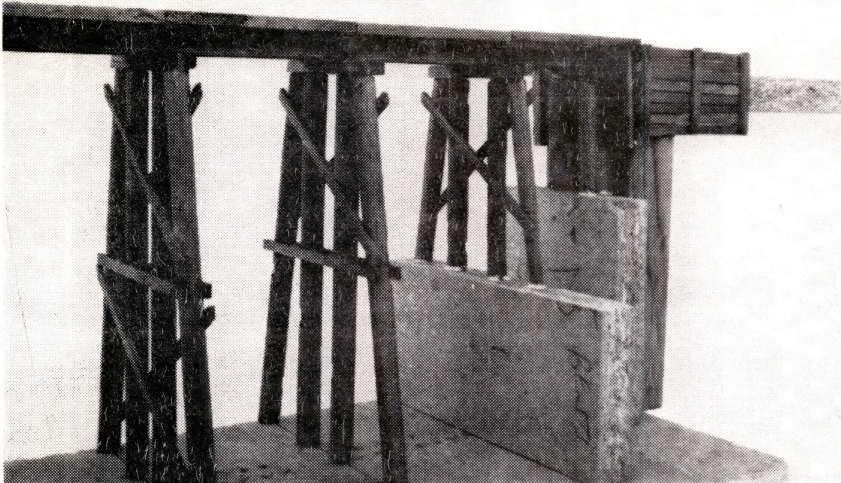
Build up your support for the scenery and, if using plaster, be careful not to slop it over your carefully stained timber. Mix the plaster in a runny mixture and flow it in from the high side.



An unusual prototype trestle - this is on the SECV 2' tramway at Rubicon. This trestle is about to be rebuilt.

Well, that's it! You can now heave a sigh of relief and admire your train as it trundles over your handywork. It's a great feeling. Happy modelling!

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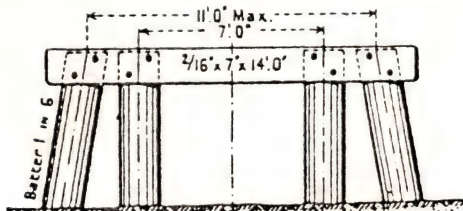


Detail of the abutment. Note the stepping down of the first and second piers.

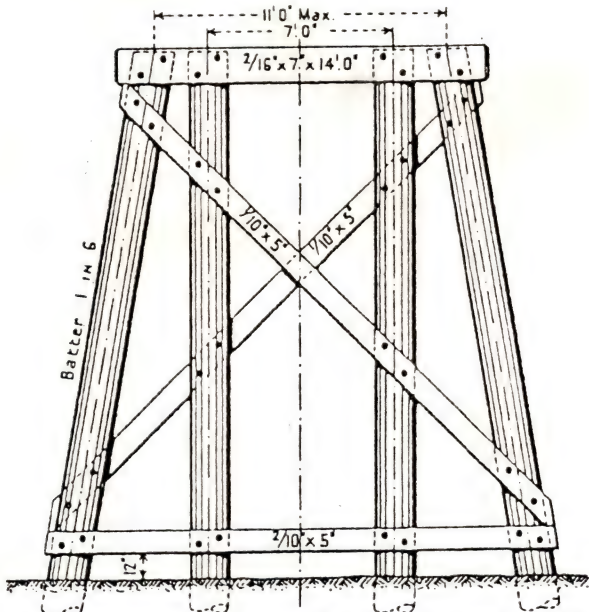
The completed trestle, ready for installation.



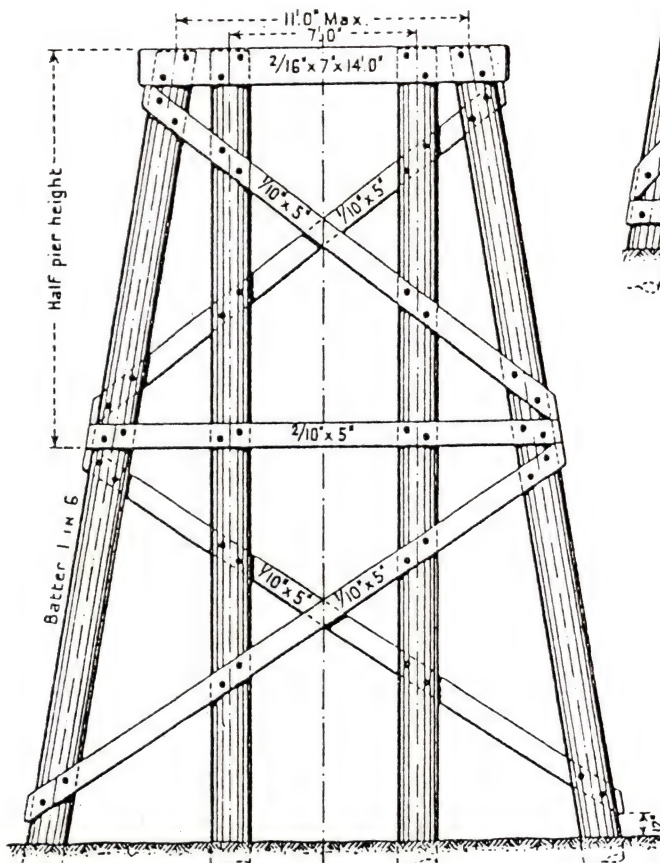




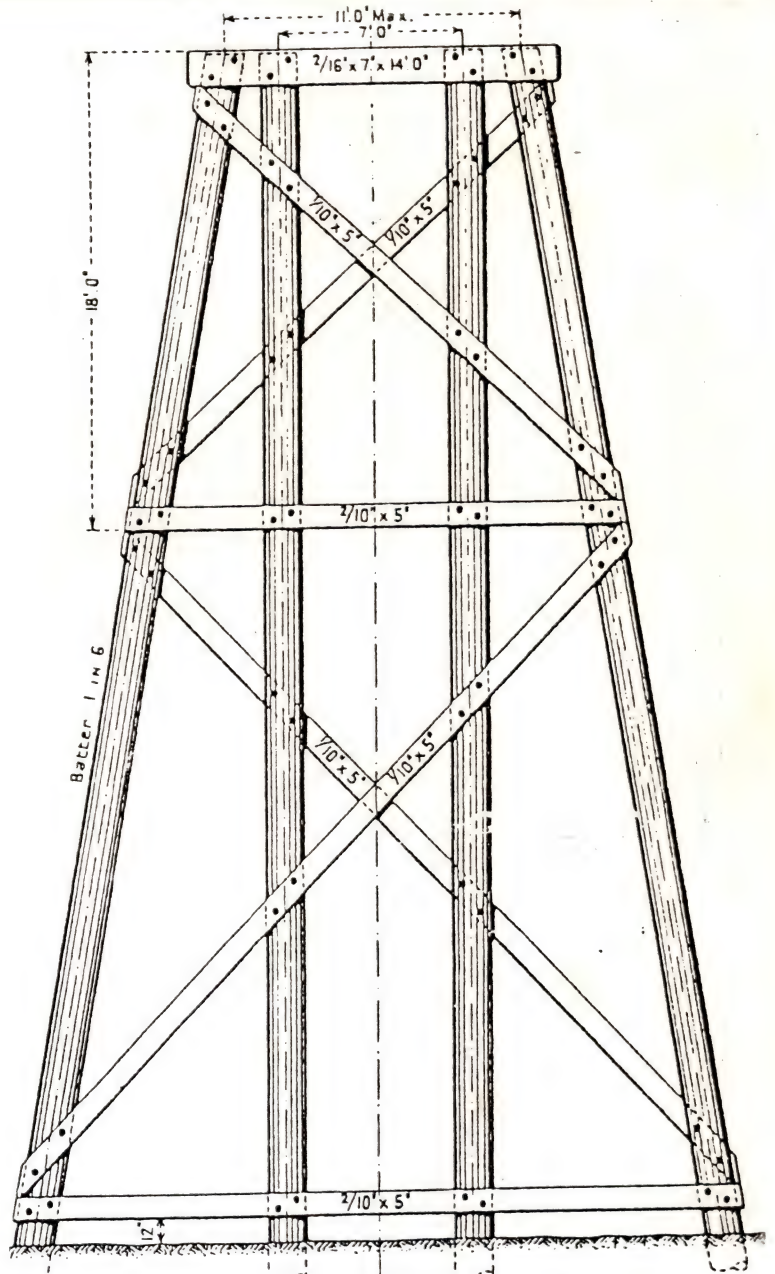
Piers up to 6.0' high



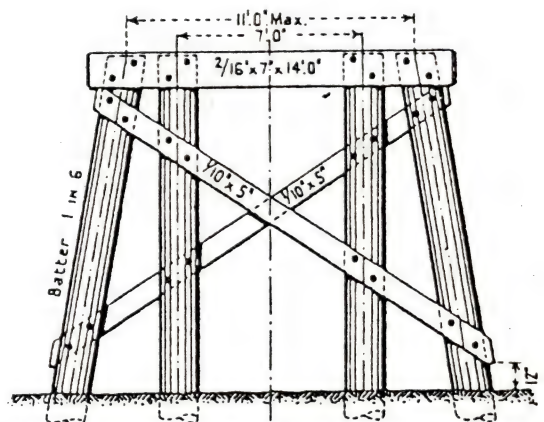
Piers over 13.0' to 21.0' high



Piers over 21.0' to 32.0' high



Piers over 32.0' to 45.0' high

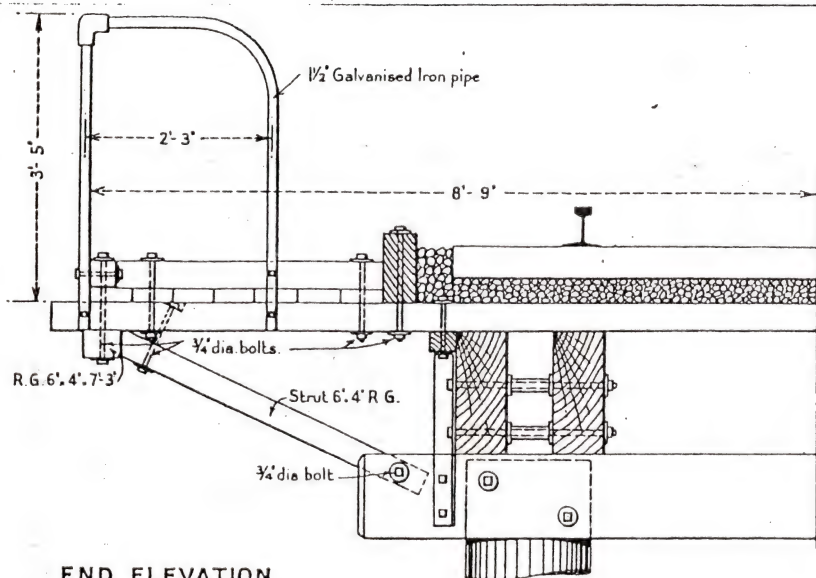


Piers over 6.0' to 13.0' high

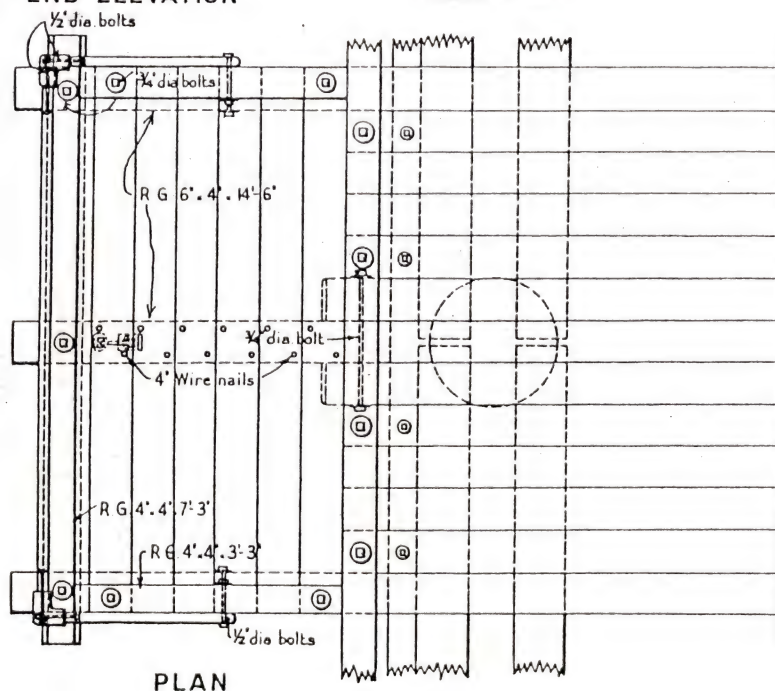
## PLANS FROM VICTORIAN RAILWAYS

The plans were supplied by Bob Dunne.



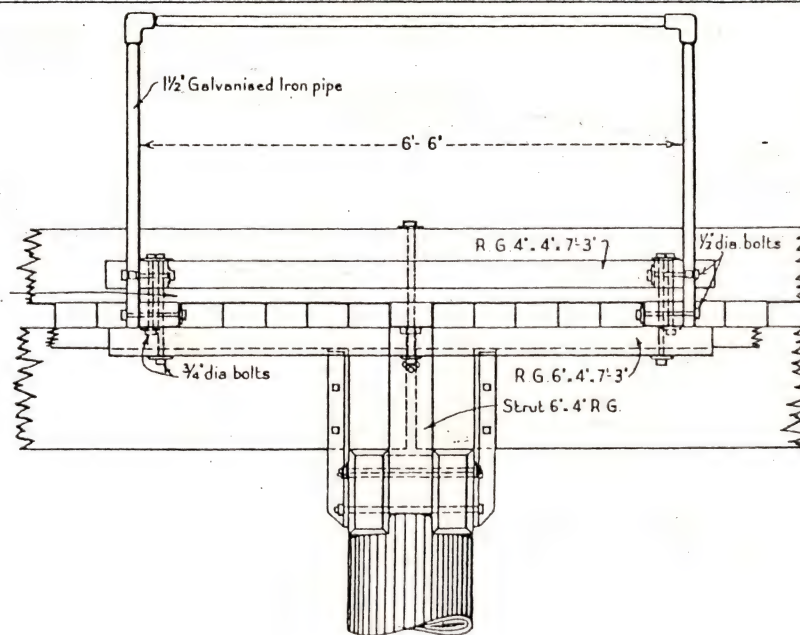


END ELEVATION



PLAN

Deck 6" x 2" x 6'-6" R.G.



SIDE ELEVATION

# SCHEDULE OF QUANTITIES ONE BRACKET

DESCRIPTION	QUANTITIES
Red Gum 6' x 4'	3/14' 6' 1/7' 3' 1/6' 0'
• 4' x 4'	1/7' 3' 2/3' 3'
• 6' x 2'	7' 6' 6'
Bolts 3/4" dia	5/12' 3/16' 1/16' varies
Washers for 3/4" bolts	18 No.
Bolts 1/2" dia	4/9' 4/7'
Washers for 1/2" bolts	8 No.
Wire Nails 4"	11b
1 1/2" dia G.W.I. Pipes	1/6' 5' screwed both ends
• • •	2/5' 4' curved & screwed one end with 2 holes for 1/2" bolts
• • •	2/3' 7' screwed one end with 2 holes for 1/2" bolts
1 1/2" Elbows - 3 way	2 No.

## NOTES

Safety brackets to be erected in general on up stream side of bridge. Length of 6' x 4' strut & angle at which it is placed must be adjusted to suit piers.

This plan supersedes plan No F.266.

VICTORIAN RAILWAYS-WAY & WORKS BRANCH

## STANDARD DRAWING SAFETY BRACKET

Approved  
Chief Civil Engineer  
Drawn by K.F.L. Checked by T.H.J.  
Engineer of Structure Design

Adopted  
June 1942  
PLAN NO  
F.266 A.







# THE NSW BRANCH EXHIBITION : 1986

## A PERSONAL REPORT FROM JACK PARKER

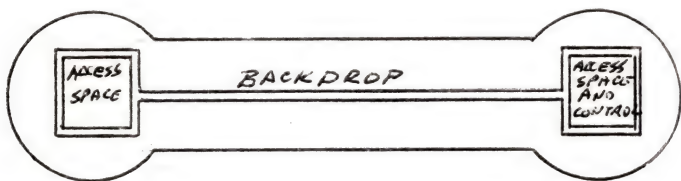
The 1986 Sydney Model Railway Exhibition was held at the E G Whitlam Recreation Centre at Liverpool, on 4, 5 and 6 October. This is quite a large venue; there was space for 54 stands, some of them being quite large.

The photographs were taken with available light, with some infill provided with an ordinary battery torch. The lighting in the hall comes down vertically, with very little side light. This makes things difficult unless stands are provided with very good supplementary lighting. Some stands had lights mounted above the backdrop, but layouts look much better (and are easier to photograph), when lights are mounted above the front edge of the baseboard.

There is no doubt that as time goes on the quality of modelling is getting better and better. There is a photo here of the station building on the NSW MRC layout; a very nice piece of modelling. This was one of the photos where I used a torch to fill in, otherwise the area under the awning would have appeared very dark. Just behind this building is Ross Hurley's knarled old gum tree. This would be about the most realistic model of a gum tree I've yet seen, with its exposed roots and blackened burnt out trunk.

The South Maitland layout showed an interesting way to represent a forested backdrop. The trunks of the trees were painted on a strip of transparent material, the top edge of the material was cut to shape and green scenic material was stuck on. The strip was mounted a little forward of the painted backdrop, giving an illusion of depth.

I've sometimes noticed that large square layouts can waste space in exhibition halls because they have large internal access spaces, but the South Maitland showed a very good space saving idea; it is shaped like this:



This shape has the potential for easy expansion by adding to the middle section.

Phil Knife's Somersetshire Midland is a small terminal to fiddle yard layout and well worthy of detailed examination. There are a couple of very nice signals; a nice touch was that the surface of the check and wing rails are a prototypical rust colour.

The NSWGR 57 Class is (or was) a 4-8-2 heavy mountain type locomotive and there was a proliferation of models at the 1986 show. The Western Districts Live Steamers had a massive 7½" gauge machine on show and also a 5" gauge example. O gauge and HO gauge models were doing their thing on various layouts. The photograph included here shows Bob Surtees' 5703 modelled entirely from radiata pine, apart from some copper wire used for piping. No, Bob does not have any extra

special woodworking machinery, he produces models like this from quite ordinary tools. He made the wheels with a hole saw, even the rails are made of wood.

There were many impressive layouts - the American prototype N gauge from South Brisbane, the Dartmouth GWR layout. There were quite a lot of layouts based on NSWGR steam era which was good to see. I remember when we first built the Hawkesbury layout (not in the '86 show), we had trouble finding enough NSW stock to run on it, but the position is entirely different now.

The Ardglen layout showed a couple of interesting new diesel locos in HO scale and Bruce Norton, AMRA's resident diesel expert, was able to provide some information about them. Of principal interest were the Australian National AL locomotives in maroon/silver and green/gold colour schemes; these models being an epoxy body on a modified Athearn chassis. They are painted and ready-to-run and should be available for about \$180 each. Another loco was the Alco S2 produced by Roco for Atlas; this model had been painted green/yellow, as per the prototype example held by the Pilbara Railways Historical Society in Dampier, Western Australia.

Along with many others, I helped with setting up on Friday and various duties on the Saturday, Sunday, Monday that the exhibition was open.

I spent some time selling tickets. At \$3.50 adults, \$1.50 children and pensioners, plenty of mental arithmetic was required, but with the help of a chart that was provided, most of the more common combinations of tickets were soon memorised. And then an Akela strolls up and says 'four adults and 27 children, please!' Instant mental panic! But no, Dave Bennett, the supervisor on duty was passing at the time and we set a special 'bulk' rate.

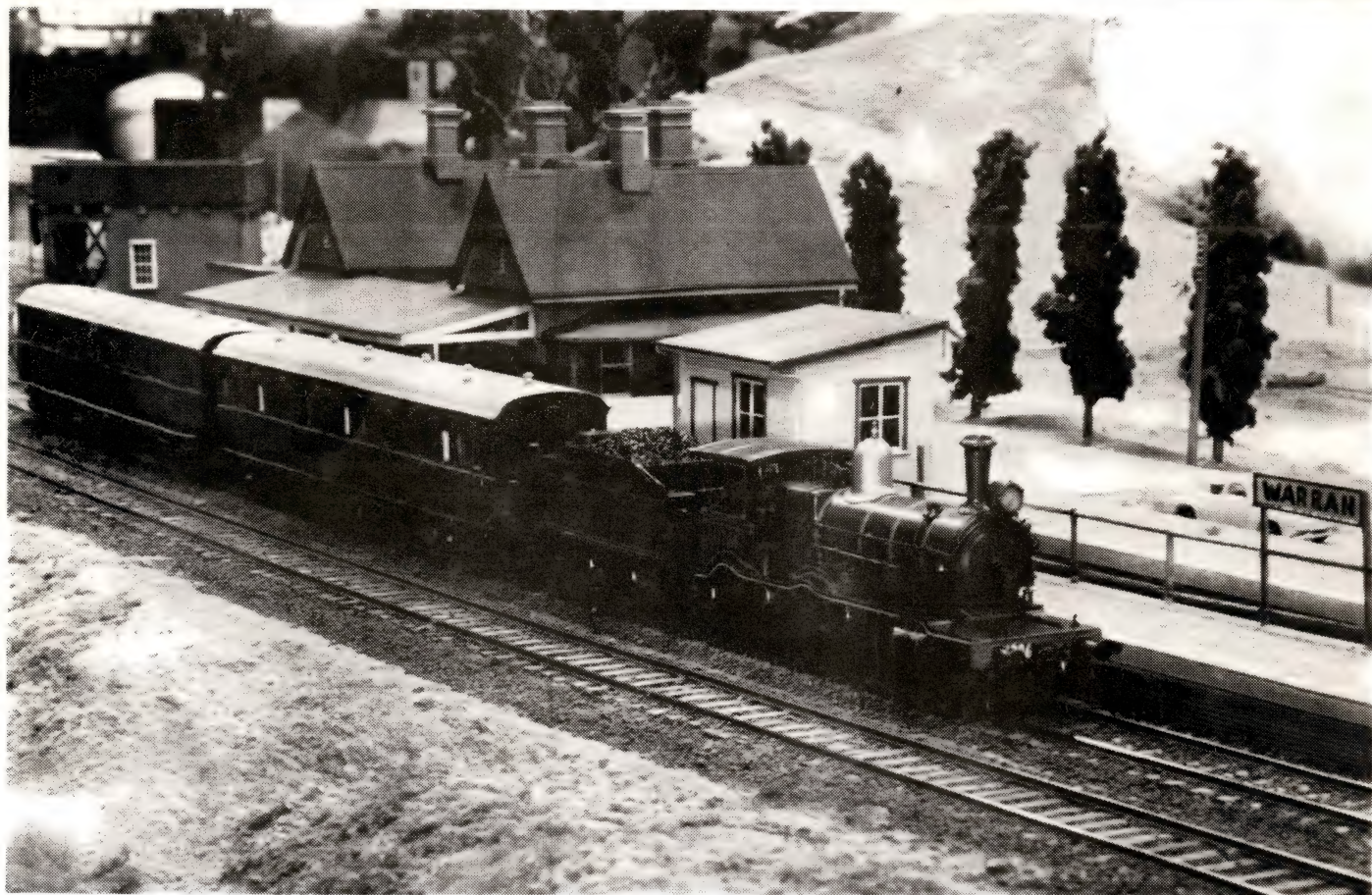
I was on the Information Stand with Bruce Norton when Bob Dunn of Broad Gauge Models approached with samples of some HO wheels they are having manufactured. The wheels are finely spoked and the disc wheels have the correct shape, but the question that needs to be asked is - why do they have the rounded RP25 contour when a design for a much better wheel contour is available in the AMRA standards? Perhaps part of the answer is that manufacturers have to produce what they can sell, so it's really up to you modellers out there to ask for the AMRA contour when you purchase wheels.

There are many people who should be given individual thanks; there is Glenn Killham for example, who organised the duty roster, Jack Treseder and Fred Stell, who ran the tea annex off the dining room. Frank Peck is always a willing worker. Then there are our valued ladies, like Kathy Knife and Sue Berrs. There's the Bunker family also; the list could go on and on.

Graham and June Larmour have decided that after this exhibition, they will be stepping down to take a break. Graham and June have organised about the last 20 exhibitions and ordinary thanks are just not enough. For the contribution they have made to the hobby they deserve our utmost respect.

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A 17 class and train pauses at Warrah on the NSW Model Railway Club's layout.

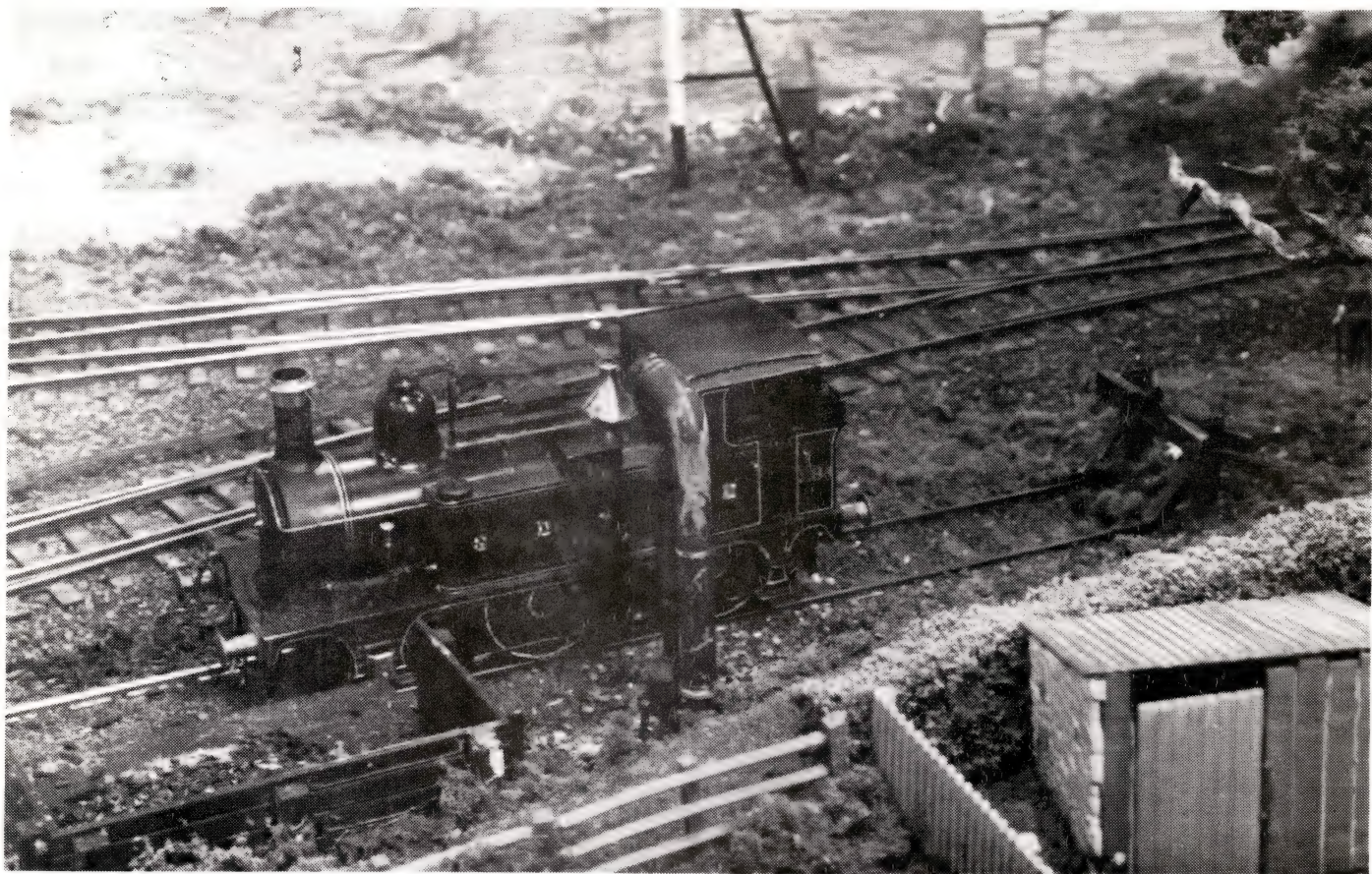


The station building on the NSW Model Railway Club's layout.



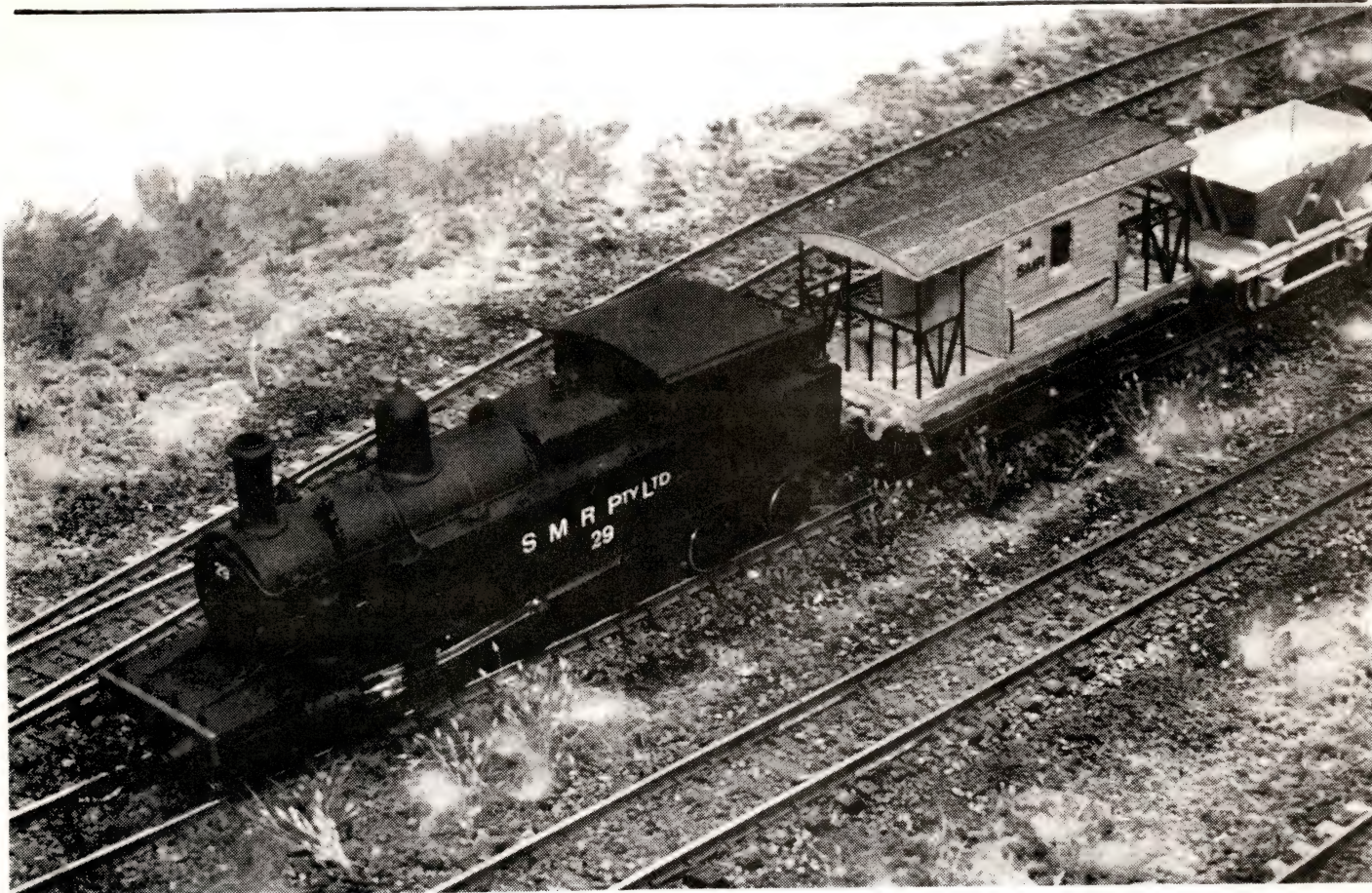


Phil Knife's Somersetshire Midland Railway.

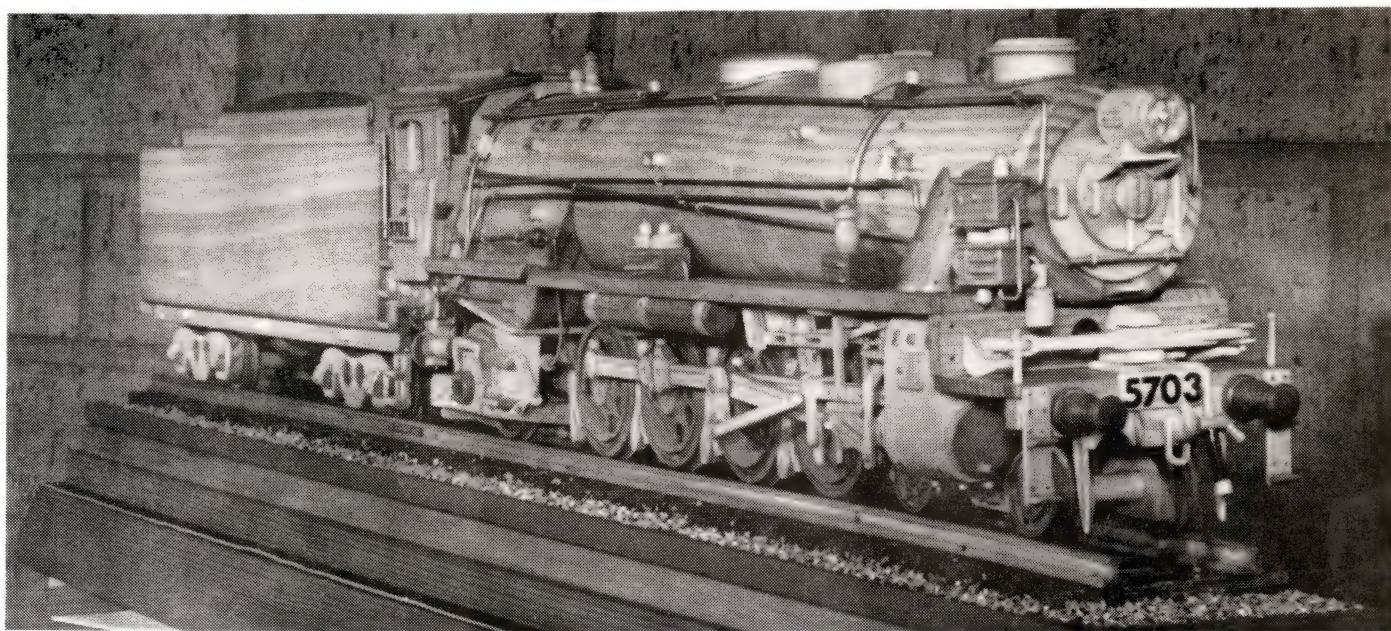


A close-up of Phil Knife's Somersetshire Midland.





HO scale layout based on the South Maitland Railway.



NSWGR 5703 in radiata pine by Bob Surtees.

\*\*\*\*\*

## WANTED

Lady penfriend wanted for a single 46 years old model railways enthusiast. Other interests include computers and social studies.

The lady in question would have to be interested in the hobby of model railways and be prepared to be involved in all facets and activities of the hobby.

I will endeavour to answer all letters.

John Ross Wilson  
29 Winnicoopa Road  
BLAXLAND NSW 2774

\*\*\*\*\*

## THE BLACK HOLE

By Ken Edge-Williams.

The Edgehill Mine is situated just down the line from the junction on a busy single tracked section. Once a day, a shunt train arrives to shunt the mine. The load is usually 8 VJM coal wagons and a BBV guards van. The only trouble however is that due to the busy nature of the branch line, the train is locked into the mine yard to enable the normal safeworking arrangement to allow trains to pass on the branch.

The crew therefore have to exchange their 8 empty wagons for 8 loaded wagons, and have the van



at the correct end and be ready to depart when a break in the branch traffic will allow it. The major problem is that the mine yard is very cramped, and although the experienced crews can do the job speedily, new crews can spend the whole shift there. How do you think you would last at the job?

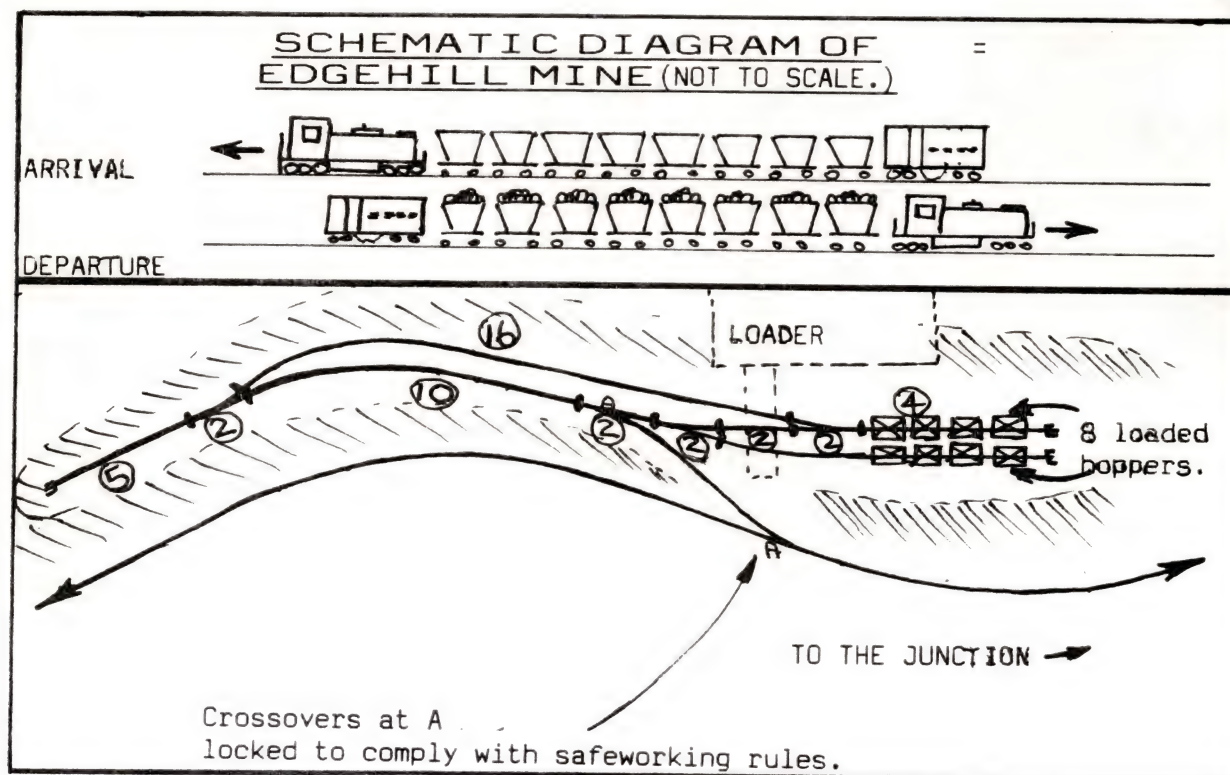
The loco is equal to 3 coal wagons in length and the BBV van is equal to 2 wagons. The VJM coal wagon is equal to one unit. The siding capacity is shown on the diagram between points, in units, and the point sections can hold two units. No fly shunting or loose shunting is allowed to occur due to the hazards it could present to the workers in the mine loading area, in other words the wagons moved must be attached to the loco when moving.

The loaded wagons are four to each track and the empties must be placed with four to each track

also. The mine has a tractor that can move no more than four at a time, but due to union rules, they cannot help the railway men with their job of shunting.

Counting each movement as 5 minutes and an attachment or detachment of a wagon signifying the start of a new move. Wagons can be left on the points during shunting, but none can be placed on the track connecting to the main line, as the points are interlocked with the branch line to prevent runaway wagons heading for the main line.

How long do you take? Remember all moves are to be legal, and no, you cannot sit in the siding, having smoko, and then ring the controller and say you have finished shunting and then get clearance to go onto the branch line to go home, do the shunting fast, leave, and then say you had a breakdown! After all the inspector is on board today.



#### SOLUTION TO BLACK HOLE PROBLEM.

The answer to the problem facing the crew is quite simple, if you make sure your 1st move is to after arriving, to back onto the siding closest to the mine loader and couple onto the 4 loaded VJMs. The sequence of events is as follows

1. Back onto 4 loaded VJMs in siding closest to mine loader
2. Attach and haul to arrival road clear of point to other siding.
3. Back train onto 4 loaded VJMs in siding.
4. Leave the loaded VJMs (8) there and haul rest of train to arrival road.
5. Back train into vacant siding.
6. Detach van and haul rest of train to the arrival road.
7. Detach all wagons and the loco goes to far end of arrival road.
8. Loco then backs via back road to van.
9. Attach van and haul it to the end of the yard.
10. Back van on to train.
11. Attach train and push onto the 8 loaded VJMs.
12. Attach last 4 VJMs as they haven't been connected yet.
13. Attach the 8 VJMs to the train. Haul train to arrival road.
14. Back loadeds into other siding.

15. Detach loadeds and haul rest of train back to arrival road.
16. Back train into spare siding.

17. Detach empties and haul van back to arrival yard.
18. Back onto loaded wagons.
19. Attach loaded wagons and haul to arrival road.
20. Detach loco and run to end yard.
21. Back via back road to empty loading siding.
22. Run onto train.
23. Attach train and back onto the empty VJMs.
24. Attach to VJMs.
25. Detach last 4 VJMs and haul rest to arrival road.
26. Back to empty siding.
27. Detach empty VJMs and back train to arrival road.

You are now ready to depart 2 hours and 15 minutes after arrival. Did you do any better? Let us know.

If you think that was easy, try this. The day before the wagons weren't all loaded and there were two extra left behind so today there are 10 loaded wagons to be picked up, how long will this take you? Remember you will have 8 empties with you. I would be interested in your solutions.

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# OLD DUBLO DOESN'T FADE AWAY

by Frank Sheeran

And exhibitions are the places that prove it. A lot of things have been said about exhibitions. Some of the more printable one-liners are - "Don't worry, members you haven't seen all year will come out of the woodwork to give a hand", "It sorts the men from the boys" or "It sorts the players from the stayers".

Allan Sheeran of Charlton, Victoria, is most certainly a man among trains at exhibitions, yet he doesn't belong to a model railway club. He comes from about 60 miles up the Calder beyond Bendigo where the dust is so thick during a drought that they don't need sand in the boxes. His nearest train shop is some 50 miles - a long way on a push-bike. It was Allan, then, who was 'kidnapped' by Peter Casper and myself to help show Peter's Hornby Dublo layout at the recent Victorian Branch open house.

Allan, who wishes to become an apprentice electrician, helped get the three-rail layout to the Clubrooms, stayed to help put it up and then dug himself into a pit and stayed there for the next two days as though his life depended on it. When the show was over and the crowd departed, there he was, with the best of them helping with the packing up. He has his own two-rail layout up at Charlton and lives alongside the railway. He is a keen photographer of the prototype and has already chalked up several shots of 'hard-to-get' locos. I mean, did you really know there was a loco called 'Bob Hawk'?

Peter's layout is a twin pit affair and walk-around controllers are used that seem very like

those employed by the Victorian Branch layout down in the basement. There are multiple controller inputs in each pit and it is possible to squeeze both control panels into one pit if required for home operation. All of which, with CAB control, makes it possible for the one operator to drive four trains at the one time.

Hornby Dublo was first introduced 48 years ago in 1938. Electrically operated points and isolating rails followed in 1939. After World War 2, when the system was reintroduced, it came fitted with Peco couplings and elec-operated uncoupling ramps quickly followed. By the end of the 50s, Dublo two-rail had been introduced and production ceased in the mid 60s. Much of the range commenced reissue as Tri-ang Wrenn late the following year and updated versions have continued to be available along with many spare parts that remain usable on original Dublo machines. Parts no longer manufactured by Wrenn, such as pick-up shoes and plungers, are being manufactured by members of the HRCA and HRCAA, respectively. The plunger pick-ups are employed on the original three-rail 8F and Castle locomotives. Other items available through the collectors' clubs include pre-WW 2 couplings, armatures adapted to suit early locos, reproduction track-cleaning wagons and a bolster wagon based on the Mk 1 BR coach underframes, promoted, but never released by Dublo. Intended future locomotives are known to have included a tender drive 9F, the drawings of which went to the Tri-ang factory and are said to have been destroyed shortly after they issued their own version of the 9F. (This probably explains the introduction of the Rovex ringfield



Peter Casper's Dublo Modrail four-road mainline layout in action at Victorian Branch's recent open house. Somehow the HRCAA chaps always manage to park their layout close to the ladies at the refreshment stand. The good looking gentleman in charge of proceedings here is Allan Sheeran. The poor kid has an uncle in common with his cousin, Graham Stockfeld. Graham managed to drag himself away from the proceedings in the basement long enough to do a shift on the prototype 'sparks', wailing away on his whistle as he passed the Clubrooms.



motor.) An ex GWR 2-6-2T was also intended along with Gresley's 'Golden Arrow'.

This latter Hornby Dublo project has been reactivated by Michael Foster who also wrote a very informative and expensive New Cavendish book on Hornby Dublo. Michael has obtained permission from the present holders of the original copyright to use the Dublo drawings, rescued from the Binns Road Liverpool factory, to produce a limited run of the 'Golden Arrow' to collector club members only. This is part of a Silver Jubilee year celebration project and the production is booked solid. If there is to be a subsequent production, it will include BR black, which was the livery worn by the majority of the class in post-war years. Like the older 0 gauge, where brass reproduced circa 20s locos and more modern rolling stock are now available, Dublo doesn't seem to be about to go away.

Like many exhibition layouts on their first showings, the Peter Casper Dublo three-rail layout fought savagely at the onset against its intrepid masters. The layout had not been scenicked and all had been packed away as Peter was in the process of changing addresses at the time of the showing. Moreover, young Allan was green to the scene and his technical back-up person had never had charge of the layout before set-up day. It's truly surprising what can be done with dry scatter material, which can be put down with pick-up for reuse in mind and yet present the logic of reality in the interim. Three cheers for Black

and Decker's little nicad vacuum cleaner and big plastic bags.

This potted saga of Dublo would not be complete without a confession from somebody whose friends had long since suspected him of deserting OO British two-rail for the giant geriatric 0 gauge 'tinnies'. I have just staggered into my den to write this after the emotional experience of double-heading a train of 50 litho-printed Dublo wagons behind a pair of the original Dublo three-rail 8F locos. My layout is compatible with the one shown here and is 24' x 9'. It was first aired many years ago at Corio MRC's exhibition and goes back there for a final shake-down in its new form on Australia Day long weekend, January 1987. Meanwhile Peter's layout is undergoing an upgrade and both will be offered to the March 1988 exhibition - the year when Dublo has its Silver Jubilee. If the two layouts are given adjacent space, they could be linked to provide a massive crowd-pleasing nostalgarama.

#### FOOTNOTE

Wanted by Peter Casper and Frank Sheeran. Dublo three-rail straight track in excellent condition. Also elec-operated points, uncoupling ramps, signals, switches and pre-nationalisation locos litho printed vans and wagons - 03 277 0827.

And have you heard about the Irish train collector who sold all his trains to finance the enlargement of his train room?

#####

## MORE MATCHBOX MODELS

This, Nonny assures me, is the last of this series that will deal with the (apparently never ending) King Series of Matchbox Models!

<u>CAT NO</u>	<u>YEAR OF INTRODUCTION</u>	<u>DESCRIPTION</u>	<u>COLOUR(S)</u>	<u>SCALE (or length)</u>
K-80	1981	Dodge Custom Van	Iridescent blue	135 mm
K-101	1975	Sherman Tank	Desert Sand	1/68
K-102	1975	M48 - AZ Tank	Green	1/74
K-103	1975	Chieftain Tank	Green (Desert sand camouflage in 1978)	1/78
K-104	1975	King Tiger Tank	Grey	1/77
K-105	1975	Hover Raider	Green and fawn	1/116
K1-06	1975	Tank Transporter with M48 Tank	Green	1/69
K-107	1975	155 mm SP Howitzer	Green	1/60
K-108	1975	M3A1 Half Track APC	Blue and orange	1/63
K-109	1975-78	Sheridan Tank	Green	105 mm
K-110	1975-78	Recovery Vehicle	Green	131 mm
K-111	1975-78	Missile Launcher	Green and red	112 mm
K-112	1975-78	DAF Ambulance	Green and fawn	95 mm
K-113	1978	Military Crane Truck	Green	156 mm
K-114	1978-79	Army Aircraft Transporter	Green	203 mm
K-115	1978-79	Army Petrol Tanker	Green	229 mm
K-116	1978-79	Troop Carrier and Howitzer	Green and black	226 mm
K-117	1978-79	SP Rocket Launcher	Desert sand	105 mm
K-118	1979	Army Helicopter	Green	150 mm

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Next to be treated will be the 'Models of Yesteryear', some of which are now serious collectors' items. To be continued in the next issue.

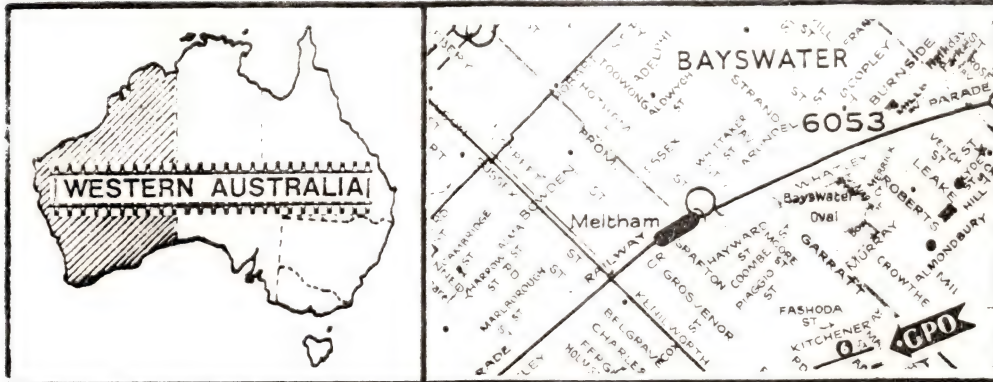
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# STATE



# NEWS



## LAYOUT NOTES

### COLORADO PACIFIC RAILROAD LAYOUT (N Gauge)

Work has been progressing steadily on the CPRR over the last couple of months. Most of the supporting timbers for the road-bed have been installed and a start made on laying the road-bed itself.

According to the program we issued in the August issue of The Branchline, we should have finished the baseboards by the end of August and have completed the track-laying by the end of November, so you can see that we are somewhat behind the program. I think it is better at this stage of the layout's construction to take our time and get it right rather than 'rush our hurdles' and potentially create future problems for ourselves. We can probably get back onto schedule later in the project.

Work progresses on the layout at most General Club Activities meetings. Anyone who is keen to help is most welcome to do so and need only check with Dennis Ling, Colin Bright or myself to ascertain what needs to be done. Hope you will be able to help.

\*\*\*\*\*

### HALTWHISTLE LAYOUT (OO Scale)

This layout is available for use by members at almost all meetings. Please note though that if you cause any accidental damage to the layout, IT IS YOUR RESPONSIBILITY TO EFFECT THE NECESSARY REPAIRS.

On the subject of damage, several people have recently expressed their concerns to me regarding the amount of damage being suffered by the Club's rolling stock. As a result, the selection of rolling stock available for use by members has been substantially reduced, the balance of the stock being stored for use at exhibitions and timetable operating sessions only.

Simon Mead  
Layout Manager

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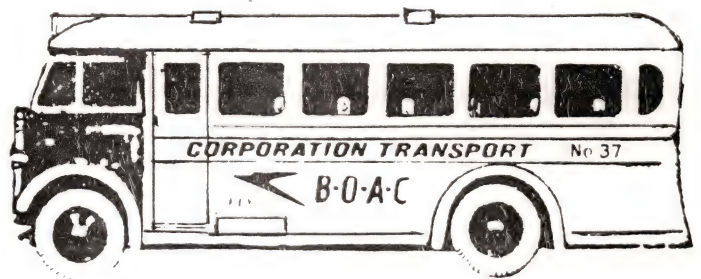
## SHOP TALK

In the June 1986 issue of The Clearing House (the quarterly magazine of the British Railway Modellers of Australia), there was a small article drawing attention to a 1930s single decker bus in the Lledo 'Days Gone' range. Although there are no indications on the model or on the packing about what prototype it represents, it is clearly of the mid to late 30s and, in many cases, lasting through to the 50s - perhaps the most popular period for railway modellers.

When I was in Sydney early in October, I paid a call on the Fantastic Toy and Model Shop in Angel Arcade (just near George Street) - the publishers of the Matchbox Buying Price List (upon which Nonny's continuing series on the Matchbox Model range is based). I went there to see whether they had any 'interesting' Matchbox models for sale, but I discovered that their buying activities of these models are based on them supplying the Sydney collectors' market and, as a result, there were none on display in the shop for sale to the public.

While I was there, I had a look around and saw on their shelves a display of the Lledo range, including the very bus which had been featured in The Clearing House. The bus is 92 mm long, 28 mm wide and 35 mm high. In 4 mm scale, the bus represents a 23' x 7' x 8'9" prototype. In 3.5 mm scale, the bus represents a 26'3½" x 8' x 10' prototype.

An almost full size photo of the bus is given here and as can be seen, it is of a type which was very common, both in the UK and also in Australia.



I am not an expert in bus prototypes and whether it represents a Guy, an AEC, a Thornycroft or a Maudesley, so I leave it to any of our members who have an interest in these things to tell me what it is. However, I did find in one of my books the dimensions of a 32 passenger omnibus supplied by Guy for the GWR in 1927. It was 25'7" x 7'6" x 9'4", so it seems that the Lledo model may be somewhere between 4 mm (OO) and 3.5 mm (HO) scales.

The Fantastic Toy and Model Shop had two different types on sale at \$8.50 each (or \$6.65 each if you bought four 'mixed' Lledo models). One was the type illustrated above - it is chrome yellow below the windows, cream above and has a light grey roof and cream coloured tyres. The other was blue below the windows and on the roof, cream around the windows and, in a lime green band along the side proclaims its owners as the Southend Corporation, whose coat of arms appears



on the side below the name. The tyres on this variant are black.

When I passed through Sydney on my way back to WA, I visited the shop of Model Cars of Australia in Kent Street. I had seen their entry in the Yellow Pages and discovered their shop to be the largest display of almost every cast metal model car ever issued which I have ever seen. Some of the models on display were definitely in the 'collector' class and one would need a very fat wallet to fill a carrier bag with purchases. However, in their display of Lledo models of 'Days Gone' were two more variants of the same bus already described. These other two were:

1 One with "Woolworth's Red" below the windows and a maroon upper section and roof. The bus has 'Oxford' on the side and just below the windows, a horizontal strip proclaiming the merits of "MORRELL'S Castle Ale, MORRELL'S Malt Stout". The tyres on this one are black.

2 One with "Woolworth's Red" below the windows and on the roof and coloured cream around the windows. On the side the bus has the "London Transport" logo. Again, the tyres are black.

The price asked for these bus models was \$6.95 each.

I think all would agree that these bus models fill a need most of us have for a bus other than a double decker, to adorn the station forecourt, awaiting the arrival of the commuter train from the city! These buses fill the bill adequately and can be 'tarted up' a bit with attention to detail, such as putting in windows, number plates, passengers, driver, etc.

The punchline to this story is that when I got back to Perth, I went into Stanbridge's Hobby Shop in Mount Lawley and what did I see - the chrome yellow 'Corporation Transport' variant of these Lledo buses. And the price - \$6.50 each! Which only goes to show that sometimes things are cheaper in Perth than elsewhere!

Alan Porter

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#### 1987 MODEL RAILWAY EXHIBITION

Only six months to go until the 1987 Model Railway Exhibition, so if you are intending to enter your layout, get cracking on any necessary work (don't leave it until next May), and let Jim Hidden know about it.

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#### HERE'S AN ODD ONE!

During my recent trip through England, Wales, Scotland and Ireland, on a visit to a Railway establishment, I saw the following notice on a carriage shed:

CAUTION

Vehicles must not  
enter this shed when  
doors are closed

BY ORDER

Now, this really got to me - how could a vehicle enter the shed with the doors closed? ..... magic perhaps.

Where did I see this sign? Dublin - no; or any other Irish town - no; Porthmadog - no; Kyle of Lochalsh - no; York - no!

It was at that 'Holy of Holies' - Didcot Railway Centre. Oh my! It is nearly enough to make you wonder about that mot!

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#### PROGRAM

#### FEBRUARY

2 Mon 'Tracklaying'

The baseboard completed, there's the track to be laid. Tonight you'll be told of some of the pitfalls to be avoided when doing this and we hope that after this talk there will be no excuses left for poor track on any of our members' layouts.

7 Sat General Club Activities

There's plenty for members to occupy themselves with on these evenings and afternoons. There's work to be done on the N gauge layout, there's the opportunity to run your own trains on the OO scale layout (16.5 mm), there's the library for you to browse in and there are other members to chat with.

11 Wed Slide Night

There must be many slides that our members have and which have not 'seen the light of day' (nor the light of a projector) for a while. If you've got some of these and you think they would be interesting for other members to see, bring along up to a dozen of them. A small prize will be awarded to the person who brings along the slide judged by those present as the best of the evening.

16 Mon Visit to a layout at Cloverdale

The members of the WA Branch have been invited to see the layout owned by a non-member who often turns up at our auctions. By all reports, this should be an interesting visit. It is located in Cloverdale and any member who wants to come on this visit must advise the Secretary by 11 February.

21 Sat General Club Activities

25 Wed General Club Activities

#### TIMES OF MEETINGS ARE AS FOLLOWS:

Mondays and Wednesdays	8 pm
Saturdays	2 pm

All meetings will be held in the Clubrooms, which are on Meltham Station, unless otherwise stated. The rooms will be open at least 15 minutes beforehand.

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#### CHARACTERS SEEN AT THE MODEL RAILWAY EXHIBITION

Two more types usually seen at any model railway exhibition, described by a biologist. Text by courtesy of Practical Model Railways.

#### SILIHATUS AMIABILIS AMERICANUS

SILIHATUS has hands which appear at first glance to be designed for crushing gauge-one Big Boy articulateds like Coke cans. Actually, they tend to be used for brain surgery, making 2 mm scale trestles from matchsticks and adjusting KayDee couplings. Every SILIHATUS has at least one Shay just to prove that all those cams and things really do work, even though his layout may represent a place 1000 miles from the nearest logging line.

If you come across an unattended American layout and you cannot decide whether it is the product of a SILIHATUS or a MANIACUS, look for the control panel. If it is hidden behind an exact-scale replica of the Rockies, expect a SILIHATUS. If it is a MANIACUS layout which is unattended, it will not have a simple cardboard clock with hands pointing to the end of the lunch



break, but a VDU telling you the same thing at enormous expense. Its control panel will be on full display, unless, as sometimes happens, MANIACUS cannot bear to be parted from it, even in the gents.



Like PARSONICUS (to be described later), the species SILIHATUS seems to be born in a state of early middle-aged degeneration; this is just as well, as a totally fit specimen would be quite capable of biting the smoke box door off the 'Spirit of St Louis' without stopping to spit out the streamlining.

#### MANIACUS ELECTRONICUS

MANIACUS may be more easily identified than any other species, in that it is the only one in which the facial hair is predominant. This sometimes tends to obscure the distinctively shaped ears, which look like that because they're ideal for tucking soldering irons behind. MANIACUS will happily run a rake of coaches marked 'Lionel Lines' up and down his 'shaped like a perverted Z Santa Fe' layout, provided that -

- 1 he can do it incredibly slowly;
- 2 the information for the spectators is supplied by computer and not by flop-over cards; and
- 3 the lights, bells and lifting barriers all work.



One MANIACUS is virtually obligatory in every model railway club; if two are allowed into the same cage, however, not only will you never get any sense out of either of them, but you will also end up with a control system nobody else can work.

This species tends to be most concentrated around towns and cities where there are major electronics manufacturers (e.g. Silicon Valley in California, Chelmsford in Essex where the Marconi facility is). There is a considerable 'export' of MANIACAE to radio controlled car, boat and aeroplane clubs, to amateur-cine and to dramatic societies throughout the world.

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## HAVE YOU SEEN ?

### Your Model Railway

October: 'Salon' - two beautiful 4 mm scale LMS Pacifics. Resin Moulding, Part 2, split and butter type moulds. Architectural modelling - a professional modeller discloses some of his secrets. Live steam locomotive construction. EXPO-S held in October. Metropolitan Junction, another look at an old favourite. Coreless Commentary looks at the RG4/RG7 motors. Truncating a prototype - East Grinstead. Layout compensation - getting better running. 'Parkwood Springs' - a 'not-so-ordinary' prototype modelled. Using Carr's sliced cork underlay. 'Camera and Comment' looks at industrial locomotives.

November: 'Salon' - two more beautiful 4 mm scale LMS locomotives - a 'Patriot' and a 'Crab'. 'Bazzing Around' Tunnel Junction, Southampton. Building an LSWR B4 0-4-OT from a Jidenco kit. Removing paint or adhesives. 'Blacksanton' the Wellcome MRS layout. 'Layout Focus' on Wessford in 0 gauge. 'Focus' type article on the MR. Deeley 0-6-44T 'Flatirons'. 'Home Etching' shows you how to produce your own kits. 'Layout Focus' on Lochend, a small Scottish terminus based on Lochty. Building the Roxey Mouldings LSWR Bullion Coach. 'Camera and Comment' looks at Sleaford, LNER.

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### Model Railway Constructor

August: Easmoor coaling stage in 4 mm scale. Variations of the BR Mk 1 coach using the Hornby/Tri-ang models as the base. 'The Generating Game' looks at small scale steam boilers. Table Top railway photography. 'Prototype Study' (a 'Focus' type of article) looks at the Hull and Barnsley Railway. The Barnum and Bailey passenger cars. 'The Black Dog' project starts on the baseboards.

September: 'The Black Dog Project' - the baseboards progress. 'Clifton Road' in TT scale. 'Datafile' looks at the Hunslet 15" x 20" 0-6-OST. Drawings, photographs and information on two of the original three Royal Coaches of the LSWR. 'The Midland Division' Part 2 - from Nationalisation to electrification. Scratchbuilding a Siphon J - outline drawing included. Drawings and photographs of a modern hopper loader at Silvertown in East London. Review of the 'Westward Models' kits of the GWR twin railcar set.

October: 'Comment' previews a major survey of railway modellers in the November issue - now's your chance to tell the 'trade' what you want, etc. 'The Black Dog Project' starts on the scenery. 'Test Report' looks at the latest Hornby 0-6-0 chassis - its hauling power is quite prodigious. 'The Generating Game' continues its look at small scale steam boilers. 'Datafile' looks at the 'Railease' POA open scrap carriers. 'Tail-and Junction' - a fine scale 4 mm layout. Plastic Teak - shows you how to model GNR and LNER teak coaches in Plasticard. Modelling canals. An LNER N2 from a Mainline body and a Perseverance chassis kit. 'Prototype Study' looks at Tool Van DW 79412.

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### Railway World

September: The history of Clifton Junction, battleground between the L&Y and the ELR fascinating reading of how bitter the railway rivalries were. Background to the naming policy for the BR Standard Pacifics and how 'Duke of



Gloucester' got its name. F W Webb of the LNWR reappraised, Part 1. 'Requiem to a Railway Ritual' - changes at Llanwrtyd Wells on the mid-Wales line - what happens when the automatics take over and the staff leave! Great Western Railway power signalling, Part 1.

October: Track diagrams, photographs and bags of information on Battersea Park loco depot of the LBSCR. Some locos that 'never were'. Part 2 of the reappraisal of the work of F W Webb. Performance of the BR Class 37 diesels.

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#### Steam Railway

August: The story of the 'rebirth' of 'Duke of Gloucester'. The Southern Railway at Exeter. A photographer looks at the railways of NE England in steam days.

September: Mallard's first run since restoration. Twilight days of the 'Britannias'. The Great Central in the London suburbs.

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#### Model Railway Journal

No 8: Upgrading the O8 diesel shunter kit by Modern Outline Kits. Canal modelling. Photographs, prototype information, drawings of 'Bamburgh', a Manning Wardle O-6-OST. SE&CR 32' 'birdcage' passenger brake from a Branchlines kit. Kit built trackwork - a look at Alan Gibson's new range of track components. 'Southwell Central' - an excellent layout in 3 mm scale. 'Small Suppliers Forum' looks at the North British Models kit for the J37, full colour signs, Waverley Borad, 4 mm coach suspension, George Norton's etched MR '700' class kit.

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#### Scale Model Trains

August: The infrastructure for a garden railway. 'Anfield' - an LMS layout in EM built to an oval shape. 'Loco Focus' looks at the GWR 4-6-0 'Country' locos, followed by an article on detailing the Dapol model. Kit bashing buildings for N scale. Flat cars and gondolas - a look at American freight cars. Building an LNER (ex GER) F5 2-4-2T loco from Plasticard. An update on developments which make EM modelling easier.

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#### Railway Modeller

October: 'Inverure' - an OO gauge layout based on the Highlands. Practical suggestions on conversion of the loft for accommodating a model railway layout. Hints on converting the Lima 4F into a 'Watford Tank' O-6-2T. A new approach to baseboard support. Drawing of the LNWR 'Watford' tank engines. Modelling the new Class 59 diesels running on BR using Plasticard, followed by a definitive article on the class with drawings, colour photographs and lots of information. Drawings of L&YR milk trucks to Diagram III. Converting the Mainline J72 to an MSWJR/GWR Dubs O-6-OT. Narrow gauge 2-6-2T of the West Clare Railway (Irish) - article with photograph and drawings. Details of the Tyne and Wear Metro system. A third article on Leam-bury MPD by Mike Cook. Modelling the SR branches.

Included in this issue is a reissue of one of the 'Show You How' booklets - this one titled 'Building Baseboards'.

November: 'Hawaby', a new layout of the Norwood MRC, using rhomboidal shaped baseboards. Converting the Wills ex LNER N7 kit to the ex

GER version and the Ian Kirk coach kits to a 'push-pull' set. Converting the Hornby Class 21/29 model into an NB D6100. Four character headcodes of BR. A summary of first attempts at model loco construction from scratch. Modelling the Southern Railway - part 6C, the models available. 'North Wirral' - an excellent layout in 4 mm of the Hull MRS. Consideration of the Cambrian Railways as a prototype worth modelling. A compact 009 layout in a coffee table based on a Derbyshire light railway. Fitting an American type drive system into a GWR. 45XX Pontesbury, a small country station near Shrewsbury. Drawings and photograph of the Barry Railway's Class H O-8-2T.

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#### Practical Model Railways

September: Details, including photographs, drawings and lots of information on Hull Paragon station. Plans for a group of four terrace cottages. A model of Wilmington Bridge, a swing bridge with signal box straddling the tracks, complete with fully detailed drawings. Making up the DJH kit of 'Duke of Gloucester'. Mamodology - the science of running a live steam engine with a minimum of fuss and equipment. 'Laneshaw-bridge' - a 4 mm layout with strong L&Y flavour. Article on rebuilding a dud model of a Vulcan Model Engineering kit of the O4 Drewry Shunter kit, but containing a lot of useful information on kit construction. Maintenance of OO diesel loco power bogies of different manufacturers.

October: Improving the Lima six-wheel milk tank wagons. Building a model of an underground toilet. Detailing the models of the BR Class 37 by Hornby and by Lima. 'The Lindesfarne and Tylwyth Railway' - description by (our) John White of his 7 mm narrow gauge layout. Building the Millholme Models kit of the LMS Ivatt 4MT 2-6-0, the 'Mucky Duck'. Building a Western Region buffer stop from rail. Converting the KeilKraft tramcar kit into a Birmingham Brill Car. Servicing the Mainline O3 Diesel Shunter. Converting a Mainline Rebuilt 'Royal Scot' back to a parallel boilered type. Modelling the London Underground.

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#### Australian Model Railway Magazine

October: Building the Campbell Scale Models kit of the Idaho Springs Goldmine. Building Victorian Railways passenger cars, part 2 - lots of photos, many coloured. Gavin Stallard's models of WAGR GE, RAM and QCS wagons in Sn3½ scale. Drawings and photographs of the standard Tasmanian Railways station building. Improving a Mehano motored diesel with new Tyco motors. Details of the Z21 and Z22 locos of the NSW, followed by an article on kit bashing a 'Roundhouse' Old Timer 2-6-0 into a Z21. Rolling stock rosters.

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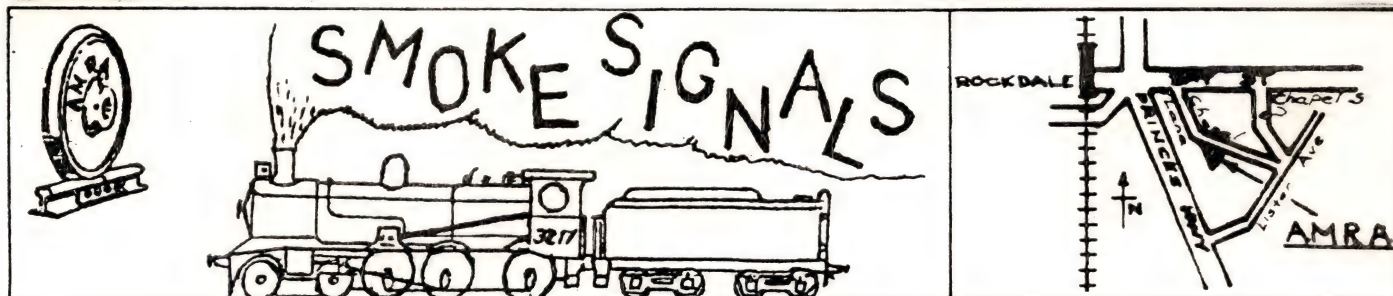
#### Railway Magazine

September: Locos of the Festiniog Railway. Resignalling the Central Wales Line. Colour photos of the new livery for 'Network SouthEast'.

October: Resignalling the Midland mainline to Leicester. Excellent colour photo of Garsdale station. Getting across Manchester without a railway and getting across London using the railways. Good colour photo of Norwich Thorpe Junction signal box.

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### FROM THE PRESIDENT

By the time you receive this copy of Smoke Signals, the Annual General Meeting will only be a short time away. It is time for you, the members, to give consideration to whom you want as President and your Committee of Management for 1987.

I am aware that some lobbying has already taken place, and there's nothing wrong with that, for your Committee should be carefully chosen. You must consider what you want in a President. To me, the President is elected to preside at the Committee of Management meetings, that is, he is the Chairman, and to fulfil that position he needs to have a knowledge of the AMRA Constitutions, both State and Federal, and of the general rules of debate. As President, he may try to guide the discussion, but cannot move motions and has only a casting vote when the voting for an against the motion is tied.

Throughout the past year, all decisions have been made by the Committee of Management and not once have I had a vote. On a couple of occasions when decisions had to be made before the next Committee meeting, those decisions were made after consultation with available Committee members and ratified at the next regular meeting. I have had it said to me that I too often ask "What is the Committee's desire with regard to this motion". The Branch Constitution simply says that a President shall be elected - it does not define his rights, privileges or authority and it is on this basis that I make my interpretation of 'President'.

Some years ago, a President adopted a more authoritarian approach, and at the following Annual General Meeting, there was much discontent and some letters of protest. If you, the membership, want to have a President with 'Executive Powers' then those powers must be clearly defined and the Constitution amended accordingly.

The President should also be in regular attendance so that he is aware of trends within the organisation and available for discussion with members on various topics that may arise. He would also normally preside over any Branch functions and be available to represent the Branch in meetings or discussions with other groups.

Another point that I would put forward for consideration is the term for which the Committee of Management is elected. Twelve months is a very short period in which to achieve any large scale project, especially if the membership is to be consulted. A case in point is the proposal for the HO layout. The decision was made at the August meeting of the COM to present for discussion two plans. Because contributions for Journal had to be with the editors within one week of that meeting, there was no time to prepare those plans, so only the announcement that the plans would be presented in the next Journal could be made. Some time then had to be allowed for members to consider the suggestions before a time could be set for a discussion of those plans. Thus there is a time lapse of approximately four months between the decision and the meeting.

Perhaps the COM should be elected for a period of two or even three years to provide a more stable management.

The Secretary is a most important part of the Committee and should also be carefully chosen. Ideally, the Secretary should attend to all correspondence speedily, draw up an agenda for the meeting and advise the President of any important matters that may be coming up for discussion.

When considering nomination of a member to the position of an office bearer or committee member, you should always ask yourself 'Is that member regular in attendance, reliable, can he contribute to the discussions and activities of the Branch?'.

BUT don't leave it all to the elected Committee. The Committee is elected to organise the activities of the Branch - not to do all the work! At the beginning of the year, I made it known that it was desired to organise visits to places of railway interest whenever a fifth Saturday occurred. Only one member came forward with a suggestion and that was not followed through.

I trust that you will consider what I have said and will attend the Annual General Meeting on 7 February, prepared to make a worthwhile contribution.

Bob Wardrop  
President

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### 'MURDOCH AND HOLMES A COURT IN SRA TAKEOVER BATTLE'

Interesting thought isn't it? Seriously though, Kerry Packer is negotiating with the NSW Government for a share in the State's railways. Rumour has it that if he's successful, he will take a band of ex-politicians and public figures to form a rebel railway network - that is - run only after dark and played under lights on the SCG. Channel 9, of course, has screening rights, with David Hill and Neville Wran leading the commentary team.

Speaking of railways and politics, the 'return' to service of 3801 was nearly as good a stunt as the arrival of the first XPT power car some two weeks prior to a State election. Fact of the matter is that as David Hill was about to step down as SRA chief, the big wigs over there decided that a great going away present would be to recommission a pristine 3801 just two days before Hill defected to the ABC. Now, the loco was really nowhere near ready - the job to finish it on time meant a lot of corner-cutting needed to be done. The final paint was done the afternoon before the official handing over! The loco was given a one month boiler certificate - apparently someone realised that the boiler wouldn't last much longer than that before packing in! The loco failed several times after the handing over and is now being stripped at Thirlmere RTM where they are going to put it together properly. I hope they repaint 3801 too. I was at Thirlmere the day it arrived, and the paint job was very disappointing indeed - the colour was fine, but the finish was very patchy and coal dust had stuck



to the paint as it was still wet when the loco was fired up for the handing over ceremony. I gave them some good hints on painting the loco - brake fluid to strip the paint - if that doesn't work, soak the thing in caustic soda - then fill up the patches with body filler, sandback, polish with brasso, then spray a grey primer (self-etch grey?) before finishing off in green.

The only problem, though, how does one get decals big enough?

Closer to home, the meeting of 20 December to discuss new layout proposals was attended by about 20 members. Even though there were as many ideas mooted as there were members present, one thing was clear - the majority (and this includes those not at the 20 December meeting) appear to be favouring plan B as shown in the last issue of Journal. The main reason is that, it is said, it provides more variety for operational purposes than the alternative plan.

So, I think it is a fairly safe bet to say that some form of layout as proposed in plan B will be built.

As you read these notes, I am sitting on a beach near Fremantle watching Australia IV defend the America's Cup. Even though I wrote these notes in December 1986, I am tipping Aussie IV to take on Stars and Stripes in the defence - and, of course, for an Aussie IV victory!!

One last thing, the popular rumour floating around the Clubrooms that I am secretly married is, for the record, NOT true - it is just a rumour. Have to go now, my wife is calling.

Until next time.....

Philip

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#### COMBINED AMRA, SCMRA, SYDNEY N GAUGE MRC COMBINED SIGNALLING CLINIC Saturday 8 November

This clinic was well attended by members of SCMRA and AMRA, but strange to say not one member of the Sydney N Gauge MRC turned up.

Neil Sorensen, a former fireman, on the NSW Railways spoke first and outlined the various types of signals and their functions. He also introduced a discussion on single track working with staff and ticket, electric staff, etc. Trevor Moore of SCMRA, a signalling engineer with the SRA, continued the discussion on safe working and told of some of the advances in signalling (safe working) techniques.

After an enjoyable afternoon tea break, Bob Wardrop introduced a novel method of operating upper quadrant signals where the mechanical aspects are non-critical and the positioning of the arm is achieved using a simple electronic servo system, the adjustment of the positions being by the use of trim pots. Dave Bennett showed and described the construction of HO and N scale colour light signals using LEDs, 10 thou enamelled copper wire being used in the N scale signals.

As Graham Larmour commented, there was something to be learned by everyone, both from the prototype and the modelling aspect.

Finally, a thank you to the ladies who catered for the afternoon tea.

Bob Wardrop

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#### N SCALE REPORT

Over the past two months, astounding progress has been made. Extremely solid bracing has been added to eliminate any problems with vibration from the 0 gauge steps. The plywood basework has been extended past the second pylon up to the valley. The elevations for Harden station

building have been drawn up. The station yard has been pencilled in and most of the Peco code 83 trackwork has been laid by Dave Bennett, Neil Watson and Glenn Killham. so far all the pointwork has been Peco, but in some places we are considering making the points by hand. That should be very interesting indeed. By the next report we should have laid all the track up to the bridge approach. Once that is complete, the backdrop for this section will be constructed. In short, everything is going swimmingly.

Glenn Watson

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#### NEPEAN SUB-BRANCH

##### PROGRAM

##### JANUARY

In recess

##### FEBRUARY

7	Sat	N Gauge layout construction
14	Sat	Slide night - BYO slides
21	Sat	N Gauge layout construction
28	Sat	Annual General Meeting

##### MARCH

14	Sat	Kit bashing - Mr Phil Collins
21	Sat	HO Modular layout - test run
28	Sat	Preparation of HO layout for exhibition

##### APRIL

4	Sat	Preparation of HO layout for exhibition
11	Sat	)
12	Sun	) Exhibition at Kurmond Public School

##### MAY

9	Sat	Kit building
16	Sat	N Gauge layout construction
23	Sat	Layout operation
		68 Shadlow Crescent, St Clair

##### JUNE

13	Sat	Control units - construction
20	Sat	N Gauge construction
27	Sat	N Gauge construction

##### MEETINGS

2nd Saturday of each month - 7 pm - St Mary's Arts and Crafts Centre, Mamre Road, St Mary's.

##### N GAUGE CONSTRUCTION

14 Desborough Road, Colyton

##### ANNUAL GENERAL MEETING

St Mary's Arts and Crafts Centre

A Gould - President  
Telephone (02) 602 0098

E Bourne - Secretary  
2 Longdon Close  
SOUTH PENRITH 2750

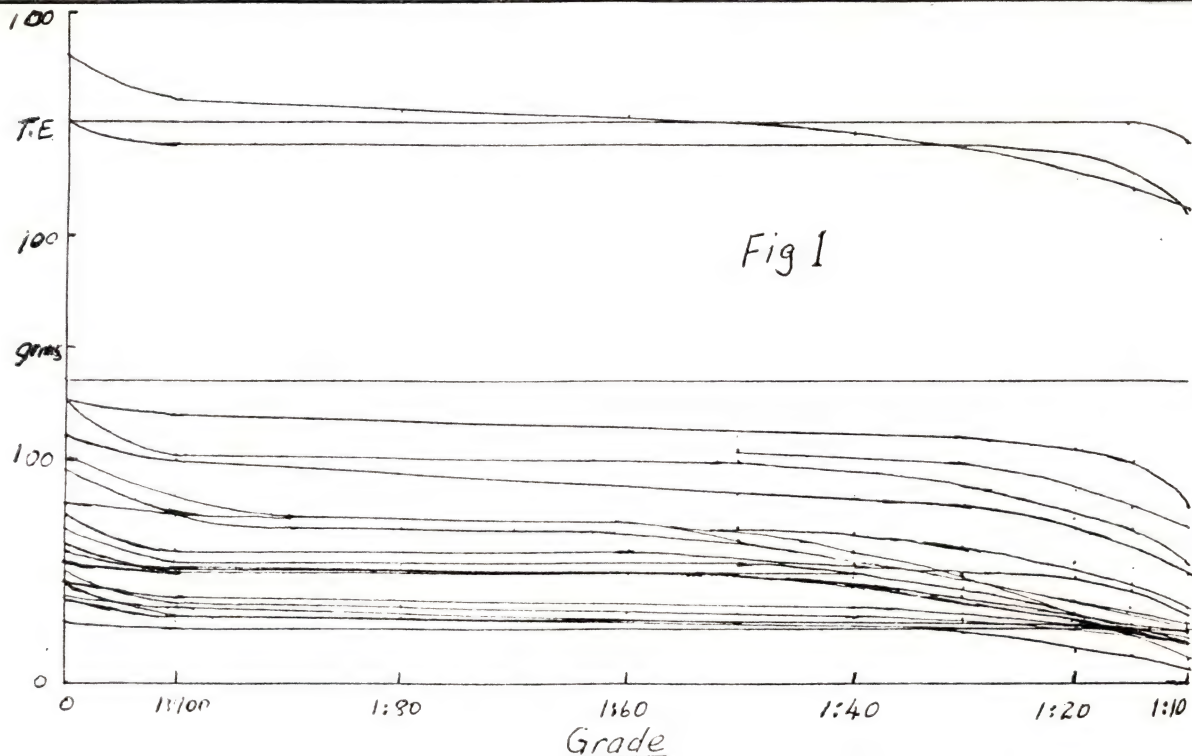
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#### WHAT WILL YOUR LOCO PULL ON A GRADE?

Saturday 15 November

Up to now, the draw bar pull competitions that I have witnessed have determined the weight that a loco can lift vertically while itself travelling horizontally, or have determined the total tractive effort by gradually increasing the grade the loco is ascending until it loses traction, the tractive effort then being calculated from the angle of the track to the horizontal and weight of the loco, portion of this tractive effort being used on a layout to lift the loco itself up the grade.





I wanted to determine the draw bar pull available to lift a train up various grades. The method used was to couple a 'dynamometer' car with a spring loaded coupling behind the loco under test. The load was adjusted by pouring dry sand into a container being lifted by a cord attached to the rear of the dynamometer car.

To adjust the gradient, pre-cut wedges of 1:100, 1:80, 1:60, 1:50, 1:40, 1:30, 1:20, 1:15 and 1:10 were inserted between two solid pieces of plywood hinged at one end. The track was set up and checked to be horizontal, using a spirit level, with no wedges inserted, and the loco with the dynamometer car coupled was placed on the track. A cord was then tied to the rear of the dynamometer car, taken over a pulley to lift a container.

To test each loco, the grade was first set at 1:10 and the loco run up the grade. Dry sand was gradually added to the container until the loco slipped. The draw bar pull in grams was read from the scale on the car and noted. The next wedge (1:15) was inserted and the test repeated. This procedure was repeated with each wedge in turn, each wedge being a slightly lesser grade. Nickel silver track was used during these tests.

With this system, any friction due to the dynamometer car or the pulley and card is also measured as part of the load.

The results of the tests are presented in graphical form in Fig. 1.

It is interesting to note that the draw bar pull, after an initial drop from the horizontal to 1:100 remains virtually constant in most cases up to a gradient of 1:30 with a few falling away after 1:50. Lightweight engines with traction tyres out-performed heavy locos without traction tyres.

The group of three curves at the top of Fig. 1 were all double motored locos, the two horizontal curves represents locos with traction tyres, while the falling curve represents an LNER Garratt built from a white metal kit and presented for testing by M Laidlay, a visitor from AMRA Victoria.

The fourth and fifth curves (from the top) depict two TRAX 48 class locos (both straight from the box). The horizontal line indicates that the loco would not slip and the draw bar pull exerted was that of a stalled loco with 15 V

d.c. applied. The other loco did slip and provided the fifth curve.

To obtain a comparison of performance with/without traction tyres and with/without additional weights, a Lima 44 class was tested as supplied by Lima, then with additional weights as supplied by Harold Warren, and finally with the additional weights, but with traction tyres removed.

The results are shown in Fig. 2.

- (1) Being the standard loco
- (2) Being with additional weights
- (3) With additional weights, but no tyres

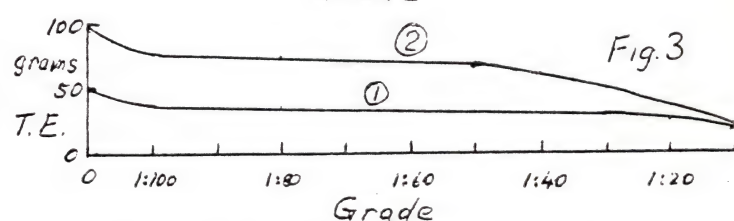
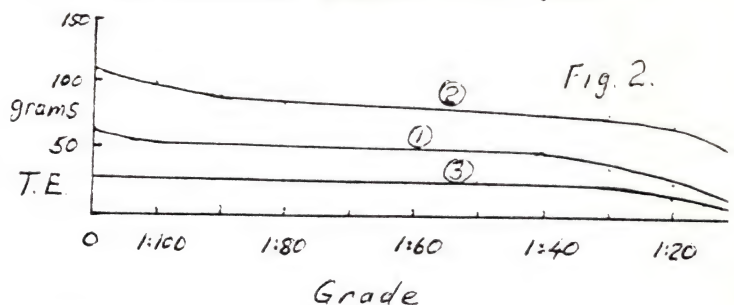
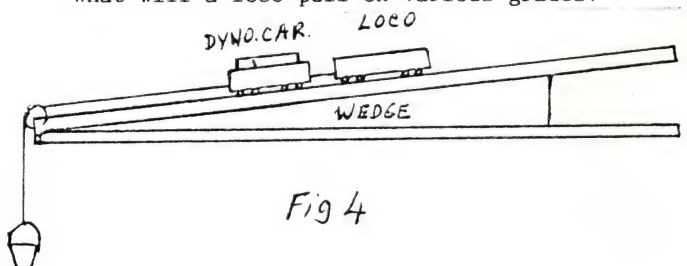


Fig. 3 shows the comparative draw bar pull of two Mansfield 38 class locos.

- (1) With nickel plated wheels
- (2) With stainless steel wheels

These tests showed conclusively that locos with traction tyres will out-perform locos without tyres and with added weight.

What will a loco pull on various grades?





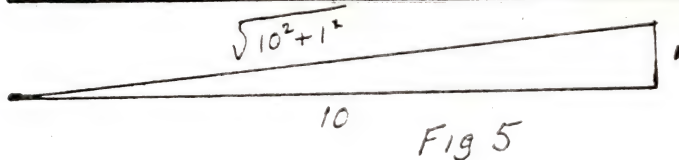
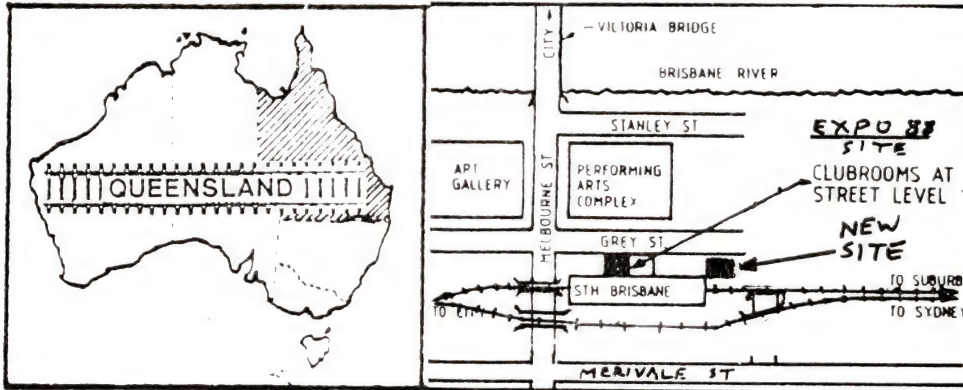


Fig 5

Using the mechanical advantage of a ramp, one unit of Tractive Effort should be able to lift considerably more up the grade than vertically. This advantage (neglecting friction) would be in the ratio of the hypotenuse over the opposite side of the triangle, as in Fig. 5.

With a grade of 1:100 this is  $\sqrt{100^2 + 1^2} = 100.005$

For a grade of 1:10 this is  $\sqrt{10^2 + 1^2} = 10.04$



#### PRESIDENT'S PARAGRAPH.

For my first piece for the Journal, I would like to take this opportunity to thank Cec Wall for the excellent job he has done as President of the Queensland Branch during the last five years.

Cec over this time has worked tirelessly for the Branch at a time of great change. At the Annual General Meeting, the members present voted to give Cec Honorary Life Membership and as one of my first official duties as President, it gave me great pleasure to present Cec with this award at our Christmas Function. Also on the same evening, Stewart Wall was presented with the Meritorious Award for 1986. I believe that Stewart is a truly worthy recipient of the award.

Jean Wall wasn't left out either, with a presentation of a coffee mug set being made to Jean.

As the saying goes "Behind every great man (or men,) stands a great woman, and Jean has been very understanding with Cec and Stewart with the amount of time and effort they have put into the Branch.

At the last C.O.M. Meeting, the committee asked Cec Wall to act as Liaison Officer with the Railways, as Cec has built up a good working relationship with the railway people over the years, and we felt it would be a shame if another member of the branch had to start the work all over again that Cec has done.

Whilst on the subject of the railways, QR has started refurbishing the lost property offices for the Branch's new club rooms. From what is being proposed by the railways, they are sparing no expense in doing the rooms up for us, with new flooring, ceilings, electrical wiring, plumbing, etc. In the Clubrooms at the moment, are some scale plans of the new rooms along with a scale rule with minimum curves etc. on the rule. A list of requirements for the layout are also available and members are invited to submit plans for the proposed layout for the new clubrooms. Plans must be drawn to scale.

That's about it from me for this issue and on closing I would like to say that I look forward to the coming year and to the challenge as the Queensland Branch President, and hope that I am

able to fulfil our members expectations,

Best wishes, Good modelling,  
Bob Mawson.

From the above, it seems that, neglecting friction, a loco should be able to lift on a grade a train having a mass of its draw bar pull, multiplied by the inverse of the gradient, i.e. on a grade of 1:100 the loco should be able to lift a train having a mass 100 times its draw bar pull.

With trains fitted with modern needle point axles and brass bearings, these results should be fairly true. This would also indicate that if a loco can pull eight cars up a grade of 1:80, it would pull six cars up a grade of 1:60.

A word of warning - don't load a loco with weights until the wheels don't slip. This is a sure recipe for burned out motors.

Bob Wardrop

able to fulfil our members expectations,

Best wishes, Good modelling,

Bob Mawson.

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#### FROM THE SECRETARY.

Applications have been distributed for the 1987 Exhibition, the Branch's tenth, Something of a milestone. Anyone not yet contacted and wishing to exhibit should contact me as soon as possible. It only seems yesterday that we staged our first exhibition at the Police Youth Club at Lang park. Members who were with us then will remember our inexperience in this area. The attendance was 600 and nearly a financial disaster. Our expertise and patronage have increased dramatically since those days. It has been decided to continue to use the Horticultural Pavilion this year. The new pavilion has some disadvantages, but will be considered for the Bi-centenary/Expo show in 1988.

I would also like at this time to welcome recently joined members, Ronald Aubrey & Frederic Oakley.

Due to the impending move in May 1987, I must request that all members holding books, magazines and plans from the Library, to please return them at an early date to enable cataloguing to be carried out. A considerable amount of material is on loan, and is causing the Librarians a great many problems. It is not intended to loan any more items until the move to the new clubrooms is completed. Also plans will not be allowed to be removed from the Clubrooms, instead the Librarian will arrange to have a photocopy made for you, due to the large missing numbers of original plans. Your cooperation would be appreciated,

Jim Christie.

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#### ANNUAL GENERAL MEETING.

On the 23rd October, the Queensland Branch members assembled for the Branch Annual General Meeting. Jim Bilby hosted the meeting in the absence of the President Cec Wall, who was on holidays in New Zealand. Following the reading of the reports the election of the office bearers for the 1986/87 period was conducted. A bit of discussion occurred over the absence of the Presidents intentions, however as there was no provision in



# SHORT TRAINS

By Ken Edge-Williams.

Modellers all over the world love the short branch line trains that exist in the real railways. Unfortunately the real railways do not, as the loss they make on most of the branch trains cause the balance books to be the wrong colour. However to modellers these trains are a blessing. Really when you think of it, how many of us have room to run

like that big layout and long trains, but instead we end up with a collection of models and no layout. So the answer is there, build a small layout and run short trains until you can afford or obtain the space to have a big layout. You will obtain more enjoyment out of this great hobby and help eliminate the armchair syndrome.

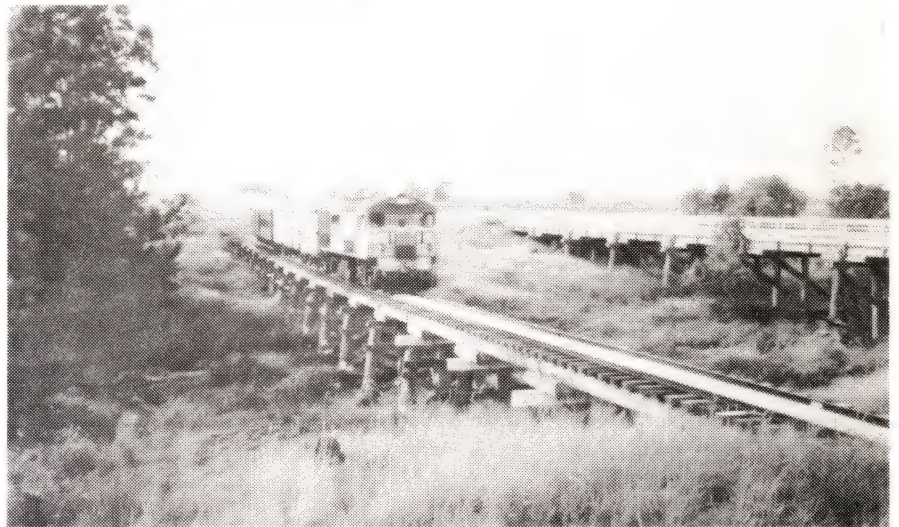


1621 with bare minimum sized train at Biloela. Definately no profit in this train today.

Photo by Ken Edge-Williams.

1600 Class Loco with short train from Thangool crossing typical Q.R. wooden bridge.

Photo by Ken Edge-Williams.



long trains, or the money to buy trains of all the same kind of wagons like you find in the triple header freights etc. These trains provide the perfect excuse for our modelling; they are short, often do lots of shunting, and have usually a variety of wagons in the consist.

Short trains means you need short passing loops, smaller yards and not many wagons. Doesn't this seem like some of the restrictions that are imposed on us empire building modellers? Another advantage is that the short trains do not have the same number of derailments as the larger ones, nor require lots of maintance.

With shorter trains, which require less wagons, you can then model the wagons with more superdetail than usually done. The same would apply to the layout, smaller with more detail than the average layout. Have you ever noticed that a lot of the well renowned layouts are not really huge? Why? It takes too long to build one and then the detail takes even longer, so unless you are a keen single modeller, or old and retired, you probably will not have the time to do that.

As the number of modellers with layouts is low, maybe this could be the reason. We all would

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'ALLO, 'ALLO

Just when you thought it was safe.....we bring back.....

HAVE YOU READ?

## HAVE YOU READ ?

by Ern Raddatz

CONTINENTAL MODELLER - May/June 1986

Achaux (Rive Gauche), Railway of the Month and a British built French layout owned by two modellers living 45 miles apart; Bridges for Model Railways, creating the need, selecting and designing bridges for layouts; Petiet 0-8-0 locos for the Nord, if you like them old and ugly.....; A Buddicom 'tenderisee', converting K's Buddicom kit to a very early French tank loco; Clochemerle en Beaujolais, another British built French layout, this time kitbashed from LGB models; Steintalbahnhof III, a German HO standard gauge layout with a very narrow gauge ambience; Une ambience francaise, French layout photos and a centre page



of 241P.11 leaving Lamballe on a Brest-Paris express; Kogin Kunkuru, railways are international and this is a layout based on the Nigerian Railways, most interesting; Russia's wandering Decapods, American built Russian engines that didn't make it to Russia; El Tortuga, HOe, British built layout that tries to capture the atmosphere of a Spanish narrow gauge railway; 'Model' friends for LGB, making personalised models of your friends; Western Maryland Pacific for N scale, converting the Rivarossi USRA light pacific; Prototype Scene-20, being a French Special this issue concentrates on France.

#### CONTINENTAL MODELLER - July/August 1986

Cumberland Valley Railroad, Railway of the Month is a gigantic N scale layout in a typical American basement; Ferrocarril de Mallorca - 1, looking at the railways of the Spanish island of Mallorca and their possibilities for a model based on them; Historic Dutch Locomotives - 5, Dutch State Railways single, a scratchbuilt HO model of an engine whose prototype was built by Beyer Peacock; Vicinal Garratt for HOe, a model of a Belgium Garratt steam tram, I wonder if we will see one on a certain popular Victorian narrow gauge layout?; Shimoizumi, a look at the way the Japanese go about railway modelling; Prototype Scene-21, this issue we look at Spain and the centre spread is a BLS (Swiss) loco depot; The Michaelmas Line, a garden layout using models based on LGB loco chassis and is narrow gauge; Stephenson Locomotive Society, a look at this well-known historical railway society, the accompanying drawing is of a cab-forward 4-4-6 tendet loco; The Big Boys - 3, third part of an interesting series, this time looking at the Rivarossi model; SMN - a French mineral railway, imagine a 30 km long railway using 10 coupled engines hauling 60 ton hoppers and you have the Societe Metallurgique de Normandie, models of the steam locos can be converted from the DB Class 44 and its diesels from Rocos Dutch diesel; Improving HO wagons, useful, but very basic.

#### CONTINENTAL MODELLERS - September/October 1986

Schorndorf, DB and EB, this Railway of the Month is an extensive British built German layout; Ferrocarril de Mallorca - 2, second part of an article looking at a Spanish island railway and its modelling possibilities; Historic Dutch locomotives - 6, Holland Railway 'Tor', looking at a Dutch 0-4-0 tank; Villeneuve, HO, the French layout of a British club, but of little interest; Build a Colonial layout, designing a freelance layout based on your favourite prototype practices, something that this writer very much agrees with; Algoma Eastern Railway, a layout based on Canadian practice; Centre spread is an ex-Nord 4-6-0 now based in England; Garratt for O gauge, a freelance Garrett using Rivarossi 0-8-0 chassis; Indian steam, an excellent article on little known, to this writer, engines; Live steam tram, using a Mamod chassis; Blitzdorf, a German style exhibition layout in N gauge; Dummy motor from scrap, a wagon load from an empty film cassette; Metre stock in N scale, scratchbuilding narrow gauge stock; Malayan Railway Class 56 Pacifics, looks like a narrow gauge SAR 620, but an attractive prototype in its own right.

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#### EUROMODEL RAIL REVIEW - 22

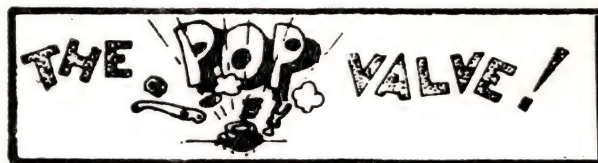
Belgian National Railways Type 27, taking a look at the prototype and its variants and undertaking a conversion/detailing project based on

the Lima model; Steam Specials, coal smoke over various European railways; From Geneva to Brig, a travelogue of a journey on a Swiss mainline; Fotoscene, this issue its on the Italian Railways; Update - DB Class 194. OBB Class 1020, an interesting look at two similar classes of baby crocodiles; Seeing the scenery, scenic ideas for railway modellers; SBB Ee 3/3, a look at an interesting Swiss shunter and its Roco model.

#### EUROMODEL RAIL REVIEW - 23

Mezidon - Then and now, a look at a French mainline junction and its modelling possibilities; Roco, a review of the Roco BB63000 French shunter; Farwell to the NS 1500s, a short history of the British Railways electric locos that emigrated to Holland and which are now withdrawn; Euro-freight, European freight trains in pictures; 1045, 1145, 1245, a look at the Austrian electrics and their Roco model; Update - DR steam, a look at a country where steam locos are still used in revenue service; On the rails, news and reviews around Europe.

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The Editor,  
AMRA Journal.  
Dear Rex,

For some time, our Qld. Branch has been looking into ways that we can help the Vic. Branch in producing the Journal. We realise we do not have the members who would love to be printers etc. up here in the north so we have no desire to take over production of the Journal.

The way we saw we could help, was in the supply of articles etc. for use, and if you look back over the past, you will see that we have been pulling our weight in this way. However we now have recently purchased a Sharp PA-1050 portable typewriter, which enables us to send down the articles and notes in the format you need for the Journal. This typewriter is a mini word processor and cost the Branch just over \$430. It has a memory and can correct errors before typing as well as having 6 styles of type at the push of a button. While not promoting this particular brand of typewriter, I feel that if all the branches arranged for similar typing of their items and notes, life would be a little easier for the production staff of the Journal. It enables a later closing date, as each Branch would be responsible for its own typing, of course with the overall editing and selection to be done by the Vic Branch.

If they don't think they can type? Well the Qld. Branch mainly has the two finger typers. With the new typewriter we have obtained, even a two finger typer can do justified and correct typing.

So how about it, Could they ease the load for the Vic Branch and shoulder some of it, and make this Association just that, a happy association of like interested modellers and 'Make Running AMRA Heaven in 87.'

Also as a matter of interest, with the new typewriter, it is quicker than typing it twice like before, as all the changes are made in the memory, so the work is actually quicker and easier, if you were typing your articles already,

K Edge-Williams.

*K. Edge-Williams*



the constitution, the election went ahead and the officers appointed were as follows:

President	None nominated.
Vice President	Jim Bilby.
Secretary	Jim Christie
Treasurer	Arthur Hayes
Committeemen	Neil Johnman
	Geoff Perkins
	Steve Malone
Librarians	Tony Weber
	Bob Mawson
Branch Sub-Editor	Ken Edge-Williams

At the next monthly meeting Bob Mawson was elected President and Marie Hill took Bob's place as a Librarian.

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#### BRANCH TIMETABLE.

Meetings continue as normal at the Branch Clubrooms, Ground Floor, South Brisbane. The Clubrooms are normally open every Thursday from 6pm and on Saturday from 12 noon.

#### URGENT

#### STOP

#### PRESS

We have been just advised by the Railways that the date for shifting our clubrooms has been advanced and most likely will be happening as you read this issue of the Journal. As a result there will be no scheduled meetings as such, although the clubrooms will be open at the normal times. You will find however that instead of running trains, you might be shifting the gear up the road. As at the time of printing the Journal, no date has been set, but we can expect a time span of only 12 weeks to do it in. Please chip in and help to do the shift, because as the old saying goes 'Many hands make light work.'

Any further information on meetings or the shifting, can be obtained by ringing the Branch Secretary, Jim Christie on Phone 345 4071.

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#### MEETING NOTES.

6/11/86 C.O.M.:— Members private property to be removed from clubrooms to enable the identification of property, etc needed to be shifted to the new clubrooms.

Sell surplus club property not required in new rooms.

Investigate the use of commercial storage needs during the shifting period to ease urgency of shift.

Listing of potential exhibitors and applications to be prepared for 1987 show. Prices for admission to be set after reference to members feelings at next monthly meeting.

Check to be made on Public Risk Insurance policy and the application to club use.

Club to subscribe to the "Australian Railways" magazine.

Discussion on new tools and equipment for club, deferred to be discussed at next C.O.M.

Future C.O.M. to be held at 6.45pm due to inability of some of the new C.O.M. members to attend at the earlier time.

Prices to be obtained for a new Branch typewriter.

27/11/86 Monthly Meeting:— N Johnman reported on the success of the recent rail motor tour to Nambour. Suggestion that S Malone be reimbursed for expenses in transporting his live steam locomotive to Nambour by road, especially as he had purchased a ticket on the train to ensure the numbers were there to justify the trip. Accolade given to Neil, and also to Steve, for their parts played in the success of the trip.

S Malone presented the prices and features of the typewriters, which he had obtained, and recommened the purchase of the Sharp PA-1050 model.

C.O.M. to be advised to discuss purchase of new, safer electric fans for use at clubrooms, especially after Marie Hill nearly lost a finger or two walking past one of our older ones.

Meeting approved the re-typed edition of Branch Constitution incorporating amendments passed at previous meetings. Also this opportunity taken to bring text into modern english.

Cec Wall advised of his decision to not stand for Club President and apologised for confusion at Annual General Meeting.

Nominations were then called for new President and Bob Mawson and Arthur Hayes nominated. Arthur advised of his inability to accept and Bob was elected unopposed.

As Bob was previously the librarian, he resigned this position and Marie Hill volunteered for the position and this was accepted by the meeting.

Bob asked Cec to continue to liase with the Railway Department and this was accepted by Cec and endorsed by the members.

4/12/86 C.O.M.:— Secretary to advise members that library property be returned to librarians to enable stocktaking.

Members to be advised also that private property to be removed from clubrooms.

R Mawson welcomed and C. Wall thanked for past service to the President's position.

N Johnman reported on success of trip and moved this become an annual event.

S Malone suggested that books of local prototype now available be purchased for library if we do not already own them. Librarian advised that books can be purchased when available if suitable and within the budget allocation.

Librarian advised that a new system for borrowing will be instituted, and that original plans will not be removed from clubrooms by borrowers although arrangements will be made to have photocopies obtained for members wanting plans for projects at home.

Discussion on disadvantages and advantages of using the new Science Pavilion for the 1987 Exhibition, and after discussion, it was decided the use of the Horticultural Pavilion be continued for 1987.

Cec Wall then outlined progress on new clubrooms. Committee agreed on the following guidelines for planning the new layout

(a) A raised layout covering the whole of the top floor area with provision underneath for storage.

(b) As an alternative to (a) a layout covering 2/3rds of the area with the remaining unused space to be used for storage.

(c) Double doors to the railway platform to be kept clear for access to storage areas.

(d) Single door to railway platform to be permanently closed.

(e) Layout design to incorporate 16.5mm, 12mm, and 10.5mm gauges with a 9mm layout to be situated underneath the raised main layout.

Discussion on supper at clubrooms and it was decided that supper to be limited to tea, coffee, and biscuits.

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#### RUNNING A MODEL RAILWAY.

As most readers know this is a pet subject of mine and as the result of visits to numerous model railways throughout the metropolitan area I am firmly convinced that the majority of these people are missing a lot of fun.

One Railway recently visited has trackwork which is really superb and fully automatic down to the last detail. Couplers worked perfectly, the models were of an excellent standard and operation was 100%, a really big thing in railway modelling.

There was only one thing wrong and that is the trains ran aimlessly. They did not perform the



duties of prototype trains.

It is disappointing to see beautifully sceniced miniature railways of which there are a few in our fair city and have to watch the trains leave the terminal one after the other and ignore every station and siding on the line. It is often the case that these same trains career on at top speed never slackening for curves or any place such as bridges, station yards, etc. where speed restrictions are likely to apply.

After all the arduous hours put in on track-laying, lining up and getting the points working, it is time then to start working out your timetables. You will have fixed in your mind the type of traffic you will be operating. You will require local freight to serve the industries along the way as well as the essentials for the storekeepers, farmers etc. along the right of way.

Fast freights such as stock trains, milk, fruit, in the specialty field, as well as a fast merchandise services will be required.

Mails have to be handled, slow or local passenger services will have to be operated and of course if you want to keep passenger business you will have to provide express passenger services.

All their trains have histories and reasons for their being run. We will in future chapters discuss the histories and reasons for the running of these trains.

It is great fun to see a team of fellows operating a layout to timetable and it is even greater fun to be in this team.

Motive power has to be rostered as on the big roads. Trains have to be made up in the yards with all vehicles in the most convenient order for shunting.

There are hosts of little intricacies which go into Railway operation which can be employed on your miniature railway to give you greater operating fun.

This above article was written in 1954 by G Parker and is fairly applicable to our Qld branch as we now have started on timetable running and have been encouraging slower more realistic speeds to improve the enjoyment of the hobby and this is over 30 years after these guys. Times are sure a changing? Maybe these 'old timers' might of known something after all?

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THE 1986 BORDER LOOP CAMP. (14-16/11/86)

This Annual A.M.R.A. Qld. Branch camp went off really well. Deb and Myself checked out the site a few weeks prior to the event, finding that all was 'as usual' and that the cows had done a good job mowing the grass. We reported and discussed the camp site at our October Monthly Meeting and there was quite a lot of interest from many who had never been before.

Deb, Myself, and Tony Weber travelled down after midday on the Friday and we were the first arrivals. We set up at our usual locations and settled in for our three days and two nights stay. Next to arrive were the Hayes family. Cloudy weather set in later in the day and occasional showers persisted throughout the night, however this did not cause any problems. Later in the evening John and Sandra Newell, Gary, Geoff, and Paul arrived followed by the Hill family thence the Wall family close behind. All then commenced to set up camp for the night.

This year the camping area was not overtaxed with only one other non A.M.R.A. family group setting up camp at the other end of the site.

The next morning the showers fell now and then, but after 10am the clouds cleared for a very fine remainder to the weekend. Stewart Wall arrived on Saturday while some of the day trippers included the Johnman family with their latest

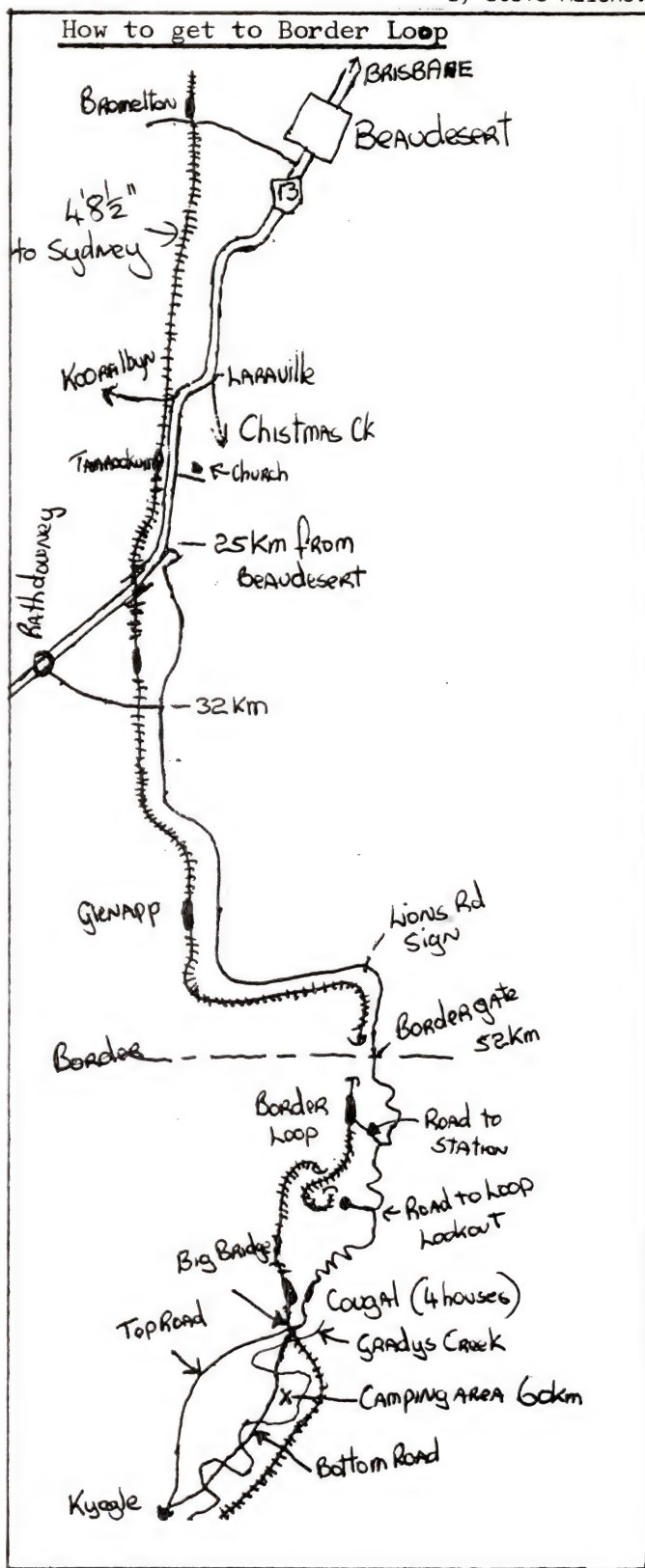
addition, Daniel (7 weeks old.)

Festivities included the usual ones; Mountain climbing, Train spotting, Train chasing, Photography, Swimming in the beautiful waterhole, Cricket, etc., in all a good family fun time. Saturday night was the best part, I feel, as the bright moonlight lighted the beautiful countryside with some interesting conversations around the campfire also.

A good round of trains were provided by the NSW system; The goods trains on Friday being slow single loco trains while on Saturday most of them were double headed and then on Sunday nearly all were triple headed trains.

After lunch on Sunday everyone packed up and headed home after a most enjoyable weekend.

by Steve Malone.





## LIBRARY NEWS.

Could all members please return all magazine and library books belonging to A.M.R.A. Qld Branch to the clubrooms by the end of February for a stocktake and re-cataloguing. This has been bought about by the apparent discrepancy between what we do have and what should be in the library. e.g. recently, a check of all A.R.H.S. Bulletins were made and all of the Bulletins which had articles on Q.R. related topics were in the list of missing magazines. I urge all Qld. Branch members to check their collections for the following magazines plus any others belonging to the Branch.

1977 July, August.

1978 August, October, December.

1979 June, February, April.

1980 August, September, October, November.

1981 September, December.

1982 December.

1983 November.

1984 November.

Please note that all borrowing is to be curtailed after the end of February until the library is set up in the new clubrooms.

Tony Weber & Marie Hill (Librarians.)

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## NAMBOUR TOUR REPORT.

by Neil Johnman.

Saturday the 22nd of November was a fine and beautiful day, the day almost 100 A.M.R.A. Qld. Branch members and their friends gathered on Roma Street Station to travel to Nambour and return by Special Rail Car, two 2000 class Silver Bullets. This trip was the culmination of four months of planning and organizing by Neil Johnman.

At precisely 9.45am, with two bells, the Guard gave right of way, and we were off and running. This was just as advertised by the computer type destination arrival and departure signs. It looked good to see our name in lights.

We headed through the city stations. As we passed the Mayne yards and engine sheds, many heads were turned in the railmotor. I wonder why? This happened again as we passed through Nundah, while overtaking a double headed northbound fast freight. Following a brief halt at Northgate, we then had a fast run to Petrie where more members were picked up. From here it was nonstop to Nambour. The Driver, a friend of the Branch Treasurer, Arthur Hayes, had the throttle wide open most of the way except when we slowed down (to about 80k?) while passing through Landsborough so members could see Don Warn's new garage (loco shed, when wife convinced) Arrival at Nambour was spot on at 11.30am.

Most of those on board made their way to our ultimate destination, The live steam park not far from the station. A few members waited at the station for about twenty minutes to photograph the fast freight which had followed us. Meanwhile everyone else was enjoying lunch, talking to friends, and riding the live steamers. One of the first members to take a ride was Ken Edge-Williams and his boys. The Bowring family were also spotted riding the little trains. Steve Malone had brought up his live steamer and was allowing A.M.R.A. members to drive it. Don Warn drove it and pulled up at the station with a full head of steam! Robert Reeves, Ken Leitch, and John Newell were all spotted driving the locos- full speed? Meanwhile Arthur Hayes was standing in the middle capturing all the action on video. I guess there will be a few embarrassed people when he shows it at the next club video night; one of them will probably be me! John Hill was made a Honorary Guard and ended up having to watch a group of brownies on one train. John was heard to complain that he had no previous experience as all he ever had was sons. Girls were

foreign loading to him. Bert Batch was also seen to be driving the loco at full speed, seems we have a bunch of speed maniacs in the club.

At about 2.20pm, everyone made their way back to the station for our departure at 2.45pm. Whilst waiting we watched the Gympie Railmotor cross a south bound fast freight at about 2.35pm.

Once again we departed on time. The trip home was a fast one overtaking two fast freights and crossing the bulk sugar train at Caboolture. Highlight of the trip was the traversing of the tunnel where men were working, lining the interior with wet cement, a lot of which dripped onto the train and anyone who was unlucky to poke a head out. Many times in the next weeks we were reminded of our trip, when upon spotting a train, we noticed the drips of cement along the rooves.

Arrival at Roma Street was 15 minutes early at 4.20pm. A great day was had by all so perhaps we can organize another trip for 1987, so if you have any ideas please let me know. Members were quite satisfied with their outing at a cost of only \$12 each or \$30 for a family.

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## STATION TALK.

Well a lot of events have been happening around the Queensland Branch lately, We have had quite a few visitors, one of the more special ones was Jack Treseder from the Victorian Branch. Jack commented on the dramatic changes in the club since his last visit more than 10 years ago.

Steve & Debbie, Garry, Paul and Jeff went camping one Friday night at Don Warn's property at Landsborough, right next to the main northern line. They saw a few trains, especially the shunt train at 1am! Who wouldn't notice a QR shunt train at that time? - 10 metres or so from their beds. Heard that they played cards on a site a few feet from the line on the Saturday morning and got sprung by the Sunlander. Unusual sight for the passengers, to see some 'Train Nuts' playing cards and sipping coke along side the track!

The stork arrived again with Geoff and Naomi Perkin's arrival of a baby daughter, Sarah Louise, on the 12/12/86 at 3.45am. Birth weight of 9lb 14oz, a large scale version it seems. Congratulations are in order as it was a case of third time lucky. Our club here seems to be jinxed with all babies of the members wives being boys and it has only been recently that any girls have started to appear.

Another member from the past, Mark Daught and his wife Kyre, visited with their daughter of a few months, Collette. It was great to have them along and we hope to see more of them in the future, Especially after the way Kyre 'hogged' the controller and ran flawlessly around the club. It put some of our regular operators to shame. Poor Mark was left holding the baby, literally. If Mark's plan was to convince his wife to let him come to the club more often, I think it might have backfired, as the way Kyre enjoyed operating the trains, Mark might be the one staying home, baby sitting.

John Newell showed some slides one afternoon at the clubrooms, and as the only dark spot available was the small storeroom, then guess where 13 members crowded in. Good thing none had a B.O. problem. The slides of the Ipswich area during the early 60's were very enjoyable, with many discussions occurring during and after the show.

Jerry Jirisek, from the Engineering Department of Q.R. gave an excellent talk on the new Electric Locos, but unfortunately time ran out and the rest of the talk was deferred until the February Meeting. Something well worth waiting for, especially now that Jerry will be able to include details of the New North Coast Electrics.

Tony Hughes has left for greener pastures in



Mackay, and we wish him luck in his future employment there.

Santa(Ken Edge-Williams) arrived at the Christmas Break-up Party just in time to present Cec Wall with a present for his past services to the club, and when finally Cec unwrapped the present, after miles of paper, it was a Z guage D.I.Y. Kit of an AD60 NSW Garrett. Unfortunately it was in the form of a lump of metal, with instructions to the effect that you just file off all the pieces that don't look like the AD60. After the humour died down, Don Warn recounted some of the earlier happenings in Cec's life, which to save some embarrassment will not be mentioned here.(a clue:- could our president be the Flasher of the Club?) Bob Mawson then presented the Honorary Life Membership. One of the few times Cec was stuck for words. Not forgotten, Jean Wall was presented with a set of Coffee Mugs, Not to remind her of the male members in the family, but instead, a thank you for allowing them to be such an active part in the club. Stewart Wall was late in arriving and missed the presentation to his dad, and was more amazed when called upon to receive the Meritorious Award. Seems in the family, Dad knew Son was getting an award and Son knew Dad was getting one. Pity they kept the secret too well. Both red faced when it all came out.

The CANUNGRA Trip coming up with more details available in early 87, but promises to be a beauty. It is planned to arrive mid-morning and then leave in mid afternoon to come home. A BBQ lunch will be held at Roger Kershaws place. Some of the activities will be Swimming, Touring the Army facilities there, Trying out the obstacle course, Inspecting the old Leahy's tramline, and much more. Come and see how fit you really are.

Talking about being fit, you should hear some of the conversation that occurs at the club supper times. Don Warn and Bob Mawson were recalling their youthful exuberance one afternoon, and disclosed that all was not rosy in their youth. Pigtales in the inkwell, Rotten eggs at the Headmasters son, and shooting split nibs around the

room at school, were just a few of the minor pranks. Some at the table reckon that both of them are still the same. John Hill claimed that he didn't get up to that sort of stuff as he went to a private school, but wife Marie was seen smiling at that statement, so who knows what secrets she knows!

Kerry Hayes volunteered to proof read the Journal contributions one afternoon at the Clubrooms, and was heard to remark that this was the only chance that she would get to read the Journal, or at least a piece of it before her husband Arthur. Seems the sod hogs it first when it arrives in the mail. How ungentlemanly of him.

Paul Scrivens has had a bash at constructing 2 KKB cattle guards vans. After much frustrations he has succeeded although he has a slight lean in one of them; it is typical of them anyway as the real wagons were never usually in top condition. After much convincing, Paul has decided that he will not make 10 QFC container flats, one at a time, instead he will make one good one, and mould the rest. We will then all be able to share in his modelling as the QFC is a fairly popular wagon in todays railway.

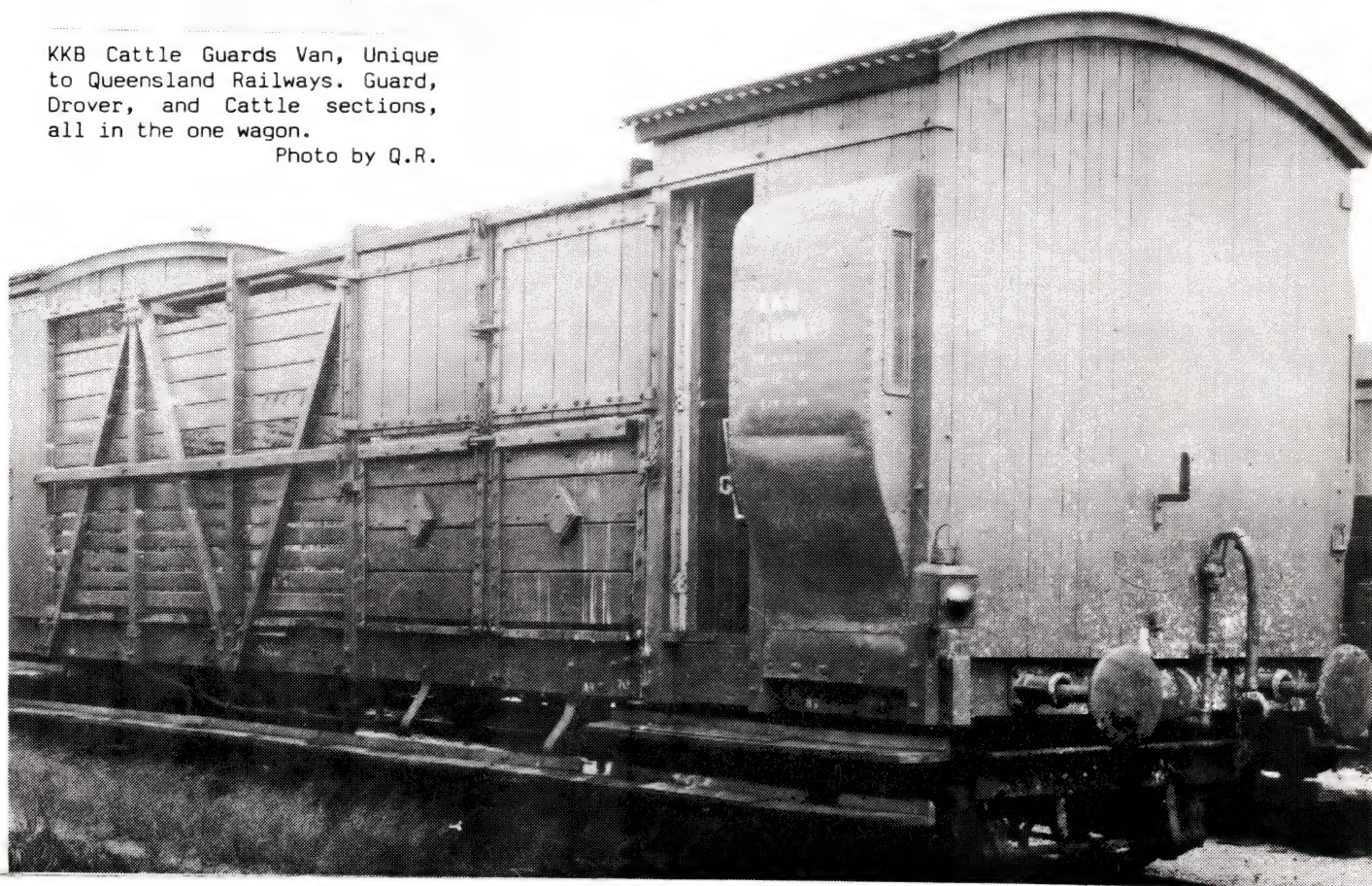
Gary Coomer has made an excellent purchase of a C17 loco and runs it whenever he gets the chance at the clubrooms. It seems that we will have another QR modeller in our ranks yet again. Although a multi-national like Gary with German, British, and Australian models, it might not be easy to pin down to one prototype.

Neil Johnman and wife Julie, took little son, Daniel for a drive one Saturday afternoon and ended up at Grandchester and did a bit of sightseeing there. Neil was quite upset to find out that a few miles further on the track were 2 Steam Trains at Laidley and they even double headed one up the range, plus there was a lot of shunting movements and the trains changed locos. Pity he found out that night that they were there. Wife Julie nearly died laughing when she found out. Seems it pays to read the notice board at the club Neil.

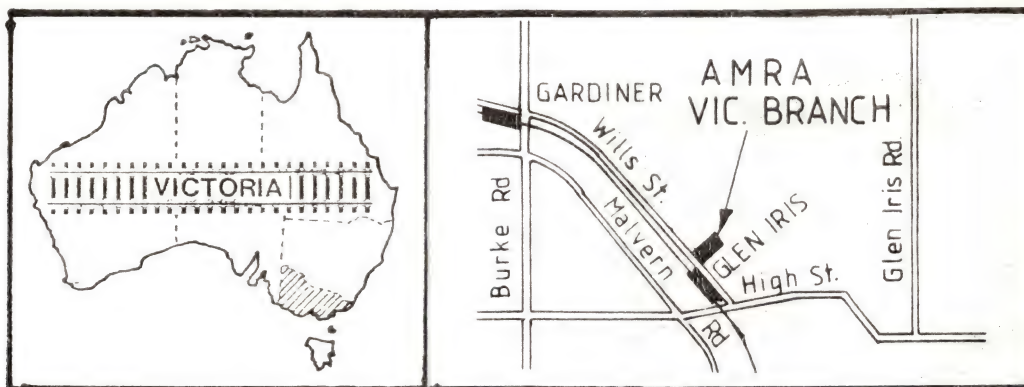
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KKB Cattle Guards Van, Unique to Queensland Railways. Guard, Drover, and Cattle sections, all in the one wagon.

Photo by Q.R.







### THE PRESIDENT'S PIECE

For those of you who were fortunate to have had an extended break from Christmas to the end of January and early February, may I say welcome back. I hope you enjoyed yourselves and took the opportunity to do some of those jobs that should have been done during 1986.

Perhaps on a slightly more serious note, may I ask the members who reside in the metropolitan area to reflect on what they can do to help this year. Assistance would be welcome in whatever capacity you can offer. Briefly, if I can indicate some of the areas that help is always needed. By far the most pressing is for helpers at the Camberwell Exhibition; the jobs are varied and if you talk to Bob Edwards or John Frost they will give you a full list of the areas that need to be covered.

The other concern is for help on the layouts, and here the choice can be yours. There is still a lot of work to be carried out on the main layout in the Clubrooms, most of the trackwork is down, however, there is a lot of landscaping to be done. In addition to this, the ModRail exhibition layout needs attention before Camberwell. The main work is a soft conversion to an English style layout with an associated change in buildings and scenery. Finally, there will be a need in 1987 for assistance with the building of our new exhibition layout 'Kyneton'. Most of the framework and some of the trackwork is being built by Bob Dunn and Jack Treseder; however, that is just the initial stage. For the rest of the layout, which includes wiring, scenery and buildings, extra hands will be needed. If you wish to help in any of these projects, come to our next meeting and talk to either Roger Lloyd, Bill Secker or Bob Dunn, they'll be pleased to see you.

For those of you who may need some inspiration, get down to the Clubrooms and see just what has been achieved with the Club layout. The Invitation Day on 22 February would be an ideal opportunity - please give it serious consideration.

With the stabilisation of the Aussie dollar at around \$0.65 against the US dollar, Broad Gauge Models look like going ahead with the C class project. If you're like me and regard the C class as a horney beast, particularly with their muffled bellow and high pitched scream from the turbochargers, then make sure you make your intentions known early, otherwise you could miss out. Remember, the G/BL class were all sold within a week of hitting the shops.

On the prototype side of things, it seems the mighty little F class 0-6-0 English Electric locos have finally succumbed to progress. The remaining few will probably have been withdrawn from service by the time you read this. Also, the operation of mainline electric locos will soon be a thing of the past. If the L class English Electric locos are still running when

you read these notes, then I strongly recommend you get a few photos of them in operation before the axe finally falls. More than likely the overhead will be dismantled beyond Warrigal with N class diesel-electrics handling all remaining traffic on the eastern railway. In addition, the remainder of the B class will be withdrawn as a major failure occurs; the same will apply to the S class. Finally, the high nose T class (347 to 356) are to be progressively withdrawn and stored.

With Christmas now well behind us, did Santa bring you what you asked for? I know one person who is a little disappointed; he asked for a brass 55 class and didn't get it. Bad luck, Roger!

Geoff Brown

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### GENERAL

November meeting was the traditional 'Flash Yours' and we entertained ourselves with a collation of slides from various members' collections. As usual, there was a world-wide collection.

There were no entries for the modelling competition.

December meeting had no syllabus item. It was party night - again an exercise in self-entertainment - bring something to share around. We did, however, have a look at a videotape on our newly acquired video recorder (courtesy Stuart Westerman - that man again) entitled 'The Ragtag Railway'; a report on the restoration of the Dorrigo line in NSW.

Presentations were made to the winner of the Observation Run on 16 November, Rob Dall. Apparently an enjoyable day was had by all, and a full report will appear in a later Journal.

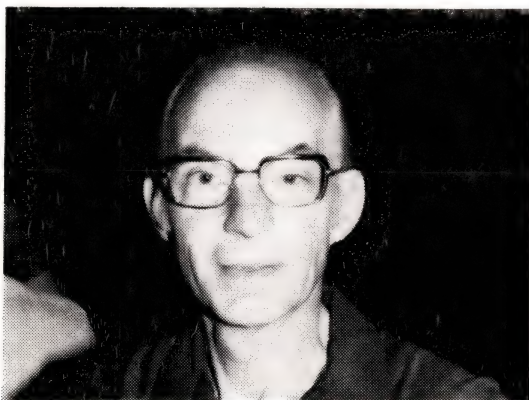
The 1986 President's Award goes to David Brown for always being available for whatever activity is in progress, cheerfully and enthusiastically.

The 1986 Meritorious Award - Federal - was made to Bill Secker. The presentation was made on behalf of the Federal Committee by Jack Treseder. The citation reads: Bill joined the Association in September 1973, and from that time he has increased his participation in Branch activities. Having observed the Branch in the mid 70s to see what it did, he has been regularly working on the Clubroom layout, as well as display layouts and working bees. Came the 80s and Bill considered he could further assist the Branch by becoming a Committeeman where, in addition to his previous Club activities, he gave further of his time on committee business.

Bill and his wife, Elizabeth, have devised and presented social activities and organised the Clubroom Open Days. Elizabeth became involved with the Exhibition meals, and, in due course, Bill was co-opted to the Exhibition Committee. Bill is the Clubroom Layout Co-ordinator, responsible to the Branch Committee for the progress



of and expenditure on this layout and the other display layouts.



### Competition Results

#### Photographic Competition

Slide Greg Attrill 95 points  
Signal lever frame, York, WA

#### Modelling Competition - Open Categories

Scratchbuilt Roger Lloyd 85 points  
Trestle bridge  
Kit or modified kit Arthur Brook 85 points  
HZN wagon

The Branch has mounted layout displays at Wagga, Malvern and Steamrail Depot (Newport) in recent weeks and the meeting's thanks to the layout crews were recorded.

Bob Marsden has offered to conduct running days for junior members on Monday 5 January and Monday 2 February 1987. Thank you, Bob

Ron Thomas

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### PROGRAM

#### FEBRUARY

- 1 Sun Timetable operation - Club (Australian)  
Rolling Stock - 1.30 pm
- 12 Thur General Meeting - Mystery Night  
Model - Kit a Australian Produced  
b Other  
Photo - Street Railway/Tram
- 15 Sun Working Bee - Clubrooms - 10 am
- 22 Sun Invitation Day - Clubrooms - 11 am

#### MARCH

- 1 Sun Timetable operation - Club (USA) rolling stock - 1.30 pm
- 5 Thur Setting up Exhibition at Camberwell Civic Centre
- 6 Fri ) Exhibition at Camberwell Civic
- 7 Sat ) Centre
- 8 Sun )
- 9 Mon ) Name on roster or pay entry fee
- 12 Thur General Meeting - Show and Tell  
Model - Open (Standard categories)  
Photo - station building
- 29 Sun Running Day - Your Equipment - 1.30 pm

#### APRIL

- 4 Sat Ladies night - 7.30 pm
- 5 Sun Timetable Operation - Club (UK) Rolling stock - 1.30 pm
- 9 Thur General Meeting - Introduction to Timetable Operation  
Model - Kit a Australian produced  
b Other  
Photo - Puffing Billy in Dandenongs
- 17 Fri )
- 18 Sat ) Easter
- 19 Sun )
- 20 Mon )
- 26 Sun Running Day - Your Equipment - 1.30 pm

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### LIBRARY NOTES

John Armstrong is always worth reading, as possibly the most experienced of the thousands of us who've designed model railway layouts, real ones for real situations, as well as dream ones for that dream train room in a dream house. He makes the point in 18 Tailor-made Model Railroad track plans (Kalmbach 1983) that many we read about, and drool over, in fact will never be built, whereas those he describes in this book have in fact been designed to fit into real rooms, with doors, windows, cupboards and (in the American context anyway) boilers and washing machines.

We may feel that just because they have been designed for someone else's house they are not relevant to us, but his introduction at any rate is certainly of interest. He starts with a series of questions we must put to ourselves before we start to design anything. The absolute key decision, the minimum curve radius, and the many others are put before us in a way that leaves us no chance of fudging the answers.

So whether we're starting to build a layout, to extend one, or even to understand why we have troubles with our existing set-up, this book is well worth consulting. Anyway, it's always fun looking over someone's shoulder.

It's a very short Library Notes this time. I've been away, and, in any case, there's not been much new offering. But there's quite a number on the way.

Watch this space.

Brian Southwell  
Librarian

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Graeme Nitz at the 1984 Ballarat Exhibition.

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### OBSERVATION RUN

In the old days these events were called Car Rallies and were rather leisurely affairs. With the coming of highly modified cars tearing around quiet country roads, the term car rally took on a new meaning. So when Graham Stockfield suggested to the COM that the Club have an Observation Run, it took us oldies a while to cotton on to what he meant.

Once the idea was explained, it was greeted with the enthusiasm it deserved. So while Graham and his wife carried out the preliminary work, the general membership were asked to keep 16 November free. The idea was to commence at the Clubrooms and end up at a park, somewhere in Melbourne, for a BBQ lunch.



A total of 12 cars took part, all with varying degree of success; in other words all made it to the correct park. A fair bit of imagination was need to get some of the clues right. My wife was navigator and, coming from a little island somewhere out in the Indian Ocean, was at a loss with some of the meanings. For example, one clue was ROCKET left down 'George' Road. My navigator furiously trying to find a George Road, until I explained it was 'Stephensons Road' we wanted. Another clue was, 'to continue along Bouncyglen Road; again confusion until I explained we were in 'Springvale' Road, and everything was okay.

Being a good day weatherwise, everyone enjoyed

themselves, and as the meeting place was Eltham Park, home of the Diamond Valley Miniature Railway, the venue was perfect. The winner was Rob Dall, closely followed by Graham Nitz and Jack and Dot Treseder.

By far the best event of the day was the sight of Elizabeth Secker charging around an East Malvern park in a bright pantsuit looking for one of the clues (an old VicRail Tie Station sign). Oh to have had a video camera at the time.

Certainly the event was a roaring success and similar ideas should be given consideration for the future. The opportunities are there, all it needs is a little imagination.

Geoff Brown

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## AUSTRALIAN MODEL RAILWAY ASSOCIATION

## NEW SOUTH WALES- MEMBERSHIP LISTING 1 NOVEMBER, 1986.

HO	FM	ABBERFIELD A.	12 Byron Street,	Cardiff North	2285	
HO	FM	ABBERFIELD D.W.	12 Byron Street,	Cardiff North	2285	54 8885
HO	OM	ABELA R.	28 Leonora Street,	Earlwood	2206	789 2696
HO	FM	AKRIGG B.B.	12 Caber Street,	Moss Vale	2577	04891 2030
HO/N	FM	ALDOUS K.	6 Noble Place,	Menai	2234	543 1119
HO	FM	ALDOUS M.A.	6 Noble Place,	Menai		
HO	FM	ALDOUS R.B.	6 Noble Place,	Menai	2234	
HO	FM	ANDREWS R.J.	36 Rainbow Pde,	Peakhurst	2210	534 5650
HO	FM	ANDREWS S.	36 Rainbow Pde.,	Peakhurst	2210	
HO	FM	ASTLE A.R.	351 Macquarie Road,	Springwood	2777	04751 2443
HO	FM	ASTLE R.	351 Macquarie Road,	Springwood	2777	
HO	SM	BAKER D.P.	1/9 Lancelot Street,	Allawah	2218	588 4846
HO(U.S.A.)	OM	BAKER J.A.	64 Ferris Street,	Annandale	2038	568 3887
O	OM	BAKER Dr. W.L.	1303 Pittwater Road,	Narrabeen	2101	913 8422
HO	OM	BAMFORD P.	21 The Bastion,	Umina	2257	04342 2655
HO	OM	BARDEN K.N.	104 Lilli Pilli Point Road,	Lilli Pilli	2229	524 5558
HO/DO-NSW	OM	BELLANTO P.J.	69 Lisgar Street,	Merrylands	2160	682 5658
N	OM	BENNETT D.R.	200 President Avenue,	Miranda	2228	524 3655
O	OM	BLIGHT J.J.	"Gum Creek", Willbriggie Mail,	Hanwood	2680	06968 5205
O/OO	OM	BOND S.	30 Arab Road,	Padstow	2211	773 3580
OO	OM	BOOKER M.N.	11 Kildare Grove,	Killarney Heights	2087	451 8807
HO/N	OM	BOURNE C.	2 Longdon Close,	South Penrith	2750	
HO	FM	BOURNE E.	2 Longdon Close,	South Penrith	2750	
HO	FM	BOURNE Mrs E.	2 Longdon Close,	South Penrith	2750	
N	OM	BOWMAN M.D.	6/110 Reserve Road,	Artarmon	2064	411 2347
OO	OM	BRADBURY K.J.	36 Nolan Street,	Berkeley	2506	04271 1841
HO	OM	BRAY G.	41 Murrumbidgee Avenue,	Caringbah	2229	524 7330
HO	OM	BROWN G.W.	"Underwood", Exeter Road,	Sutton Forest	2577	04883 4235
HO/DO/N	OM	BROWN W.G.	Cnr Bay & Davidson Streets,	Tathra	2550	06494 1316
HO(AUST)	OM	BROWN W.N.	29 Citizen Street,	Goulburn	2580	04821 7968
HO/O	OM	BRYAN J.W.	64 Helicia Road,	Macquarie Fields	2564	605 6343
HO/DO	OM	BULL F.M.	6 Ronald Avenue,	Dundas	2117	638 2404
O	FM	BUNKER I.D.	18 Sturdee Avenue,	Bulli	2516	04267 2776
O	FM	BUNKER Mrs C.	18 Sturdee Avenue,	Bulli		
O	FM	BUNKER W.	18 Sturdee Avenue,	Bulli		
HO	OM	BURKE M.W.	288 The River Road,	Revesby	2212	771 2956
HO	OM	BYRNE J.W.	245 Sylvania Road,	Miranda	2228	525 1549
HO	OM	CARSON J.B.	33 Gibsons Road,	Wollongong	2525	04271 1867
HO/HO21/2	OM	CASTLE B.	6 Jindalee Avenue,	Pt. Clare	2250	04324 1197
HO/O	OM	CHAPMAN S.	138 Railway Road,	Marayong	2148	626 9979
DO/N	OM	CHAPMAN S.J.	34 Feather Street,	St. Clair	2759	670 3252
HO	OM	CHEESEMAN J.W.	27 Fulton Avenue,	Wentworthville	2145	631 8073
N	OM	CLARKE J.F.	c/- Westpac, P.O. Box 47,	Wellington	2820	06845 2582
HO	OM	COLLINS P.H.	10 Wau Place,	Mount Druitt	2770	625 9132
HO	OM	COLLINS R.				
HO	OM	CROSSINGHAM H.	23 Clermont Avenue,	North Strathfield	2137	73 2232
HO	OM	CUNNINGHAM T.R.	P.O. Box 80,	Villawood	2163	726 5948
HO	OM	CUTTER D.W.	217 Victoria Street,	Werrington	2760	623 8569
O/	OM	CUTLER K.A.	24 Mimosa Avenue,	Toongabbie	2146	674 2502
HO	OM	DAVIES E.R.	24 Kanning Avenue,	Gymea	2227	524 1808
O	OM	DAVIES J.L.	18 Curagul Road,	North Turramurra	2074	44 1511
O	OM	DAVIES T.E.	66 Griffiths Avenue,	West Ryde	2114	80 2334
O/5*	OM	DAY E.B.	239 High Street,	Willoughby	2068	407 1173
HO/HOn3	PLM	DE HAVILLAND M.	6 Immarna Place,	Telopea	2117	683 2833
	SM	DINGLE C.J.	17 Dunwell Ave.,	Loftus	2232	521 2941
HO	OM	DRUMMOND R.J.	48 Westminster Street,	Schofields	2762	627 3623
HO	PLM	DUNN Mrs J.	26 Napoleon Street,	Rosebery	2018	667 1542
HO	PLM	DUNN J.B.	26 Napoleon Street,	Rosebery		" "
HO/DO	OM	EADES P.J.W.	14 Desborough Road,	Colyton	2760	623 5672
HO	OM	EDMOND G.L.	18 Woodhouse Drive,	Ambarvale	2560	04626 2617
HO/N(NSW)	FM	EDWARDS K.L.	8 Easton Road,	Berowra Heights	2082	456 2868
HO/N	FM	EDWARDS P.	8 Easton Road,	Berowra Heights		" "
HO	PLM	ELLIS D.F.	110 Marshhead Drive,	Hurstville Grove	2220	
HO(NSW)	FM	ELLIS G.M.	8/106 Kiara Road,	Miranda	2228	528 2009
HO	FM	ELLIS S.M.	8/106 Kiara Road,	Miranda	2228	
HO	OM	ENGLISH A.	31 Ainsworth Street,	Leichhardt	2040	569 8627
HO	OM	FLYNN P.F.	4 Emerald Place,	Murwillumbah	2484	06672 1329
	OM	FOYEL S.	43 Derribong Crescent,	Bangor	2234	
HO	OM	FORDHAM L.A.	6 Bambara Place,	Baulkham Hills	2153	639 9106
HO	OM	FOTHERINGHAM J.	4 Clucas Road,	Regents Park	2143	649 1560
HO	OM	GALLAGHER R.A.	P.O. Box 4,	Little Bay	2036	661 4046
HO/O	OM	GARDE E.N.	51 Wilson Way,	Blaxland	2774	04739 2861
OO/HO	OM	GERARD P.R.	22 Glenshee Place,	St Andrews	2566	603 2817
N	OM	GIBBINGS J.L.	99 Bettington Road,	Dundas	2117	630 1801
OO	OM	GIBSON C.W.H.	147 Barrenjoey Road,	Ettalong Beach	2257	04342 3267
HO	OM	GIOIA R.	5 York Street,	Rockdale	2216	597 1930
HO	OM	GOLDRICK Miss M.R.	85 Connell's Point Road,	South Hurstville	2221	546 2433
HO	FM	GORE R.	13 Raggatt Crescent,	Edgeworth	2285	58 5417
HO	FM	GORE T.	13 Raggatt Crescent,	Edgeworth	2285	" "
N	HLM	GORRELL R.W.	77 Boundary Street,	Roseville	2069	
HO	OM	GOULD A.K.	32 Meldrum Avenue,	Nt Pritchard	2170	602 0098



HO	OM	GOULD N.J.	28 Erang Street,	Carss Park	2221	546 4980
HO	OM	GRACE R.C.	9 Duneba Drive,	Westleigh	2120	848 9867
HO	SM	GRAHAM C.	12 Homelea Avenue,	Panania	2213	773 4503
HO	OM	GRAHAM E.G.B.	P.O. Box 388,	Caringbah	2229	524 1822
O-5"	OM	GRAHAM J.B.	P.O. Box 150,	Mortdale	2223	57 6484
HO	OM	GREEN F.	120 The Boulevard,	Caringbah	2229	524 8822
O-5"	OM	GREEN J.H.	"Mungana", Hawkesbury Road,	Yarramundi	2753	04776 1176
HO/N	OM	GRIFFITHS J.	248 Gynea Bay Road,	Gynea	2227	525 6201
HO/	FM	GUEST M.V.	31 Birch Street,	St Marys	2760	623 8780
HO	FM	GUEST S.K.	31 Birch Street,	St Marys	2760	
HO	OM	GUNTON A.	1 Stoney Creek Road,	Beverly Hills	2209	53 5891
HO	OM	HALCROW J.E.	8 Cooina Place,	Bilgola Plateau	2107	918 0774
HO	OM	HARDING A.	31 Drama Avenue,	Kirrawee	2232	521 6666
HO	OM	HARRIS A.	10 Darryl Place,	Gynea	2227	525 8987
HO	OM	HENRY S.	15 Rainbow Crescent,	Kingsgrove	2208	78 5867
O(TRANS)	OM	HERBERT C.C.	4/152 Derby Street,	Penrith	2750	04731 5106
O	OM	HETHERINGTON A.G.	38 Wolger Street,	West Como	2226	528 8138
	PLM	HILLEBRAND P.	9/19 Livingston Avenue,	Botany	2019	
Z	OM	HILLMAN B.	28 Ravenna Street,	Strathfield	2135	642 1333
ALL	GM	HILLS MODEL RAILWAY SOC.	P.O. Box 555,	Castle Hill	2154	
O	OM	HINDMARSH E.G.	48 Coolaroo Road,	Lane Cove	2066	427 6462
HO	FM	HOGAN E.	32 Dolans Road,	Cronulla	2230	523 4085
HO	FM	HOGAN Mrs V.n	32 Dolans Road,	Cronulla	2230	
HO	FM	HOGAN V.J.	26 Woodward Ave,	Caringbah	2229	524 9022
HO	FM	HOGAN Mrs T.	26 Woodward Ave,	Caringbah	2229	
OO	OM	HOUGHTON R.F.	109 Sylvania Road,	Miranda	2228	522 8010
HO	OM	HOWARTH I.B.	6 Tree Top Crescent,	Port Macquarie	2444	06583 8075
O	FM	HOWARTH P.B.	8 Milford Road,	Miranda	2228	524 5612
HO/O	FM	HOWARTH R.L.	8 Milford Road,	Miranda	2228	" "
OO	OM	HUNT P.A.	43 Glover Street,	Mosman	2088	908 2464
O	OM	IMER G.A.	42 Penfold Street,	Eastern Creek	2766	625 9885
HO	OM	JAMES R.R.	P.O. Box 736,	Grafton	2460	06642 2168
OO	FM	KEEP J.J.	3 Hercules Ave.,	Padstow	2211	772 1240
OO	FM	KEEP D.	3 Hercules Ave.,	Padstow	2211	
HO	OM	KELLY G.L.	P.O. Box 156,	Pendle Hill	2145	631 4483
HO	HM	KELLY P.B.	20 Lee Street,	Condell Park	2200	705 5317
HO	OM	KENDALL J.S.	2 Gibson Place,	Blacktown	2148	622 8153
HO	OM	KENT L.	24 Letters Street,	Evatt	2617	06258 6133
HO	OM	KILLHAM G.S.	13 Avenel Road,	Gynea Bay	2227	525 2802
N	FM	KING K.	5 First Street,	Booragul	2284	04958 6775
N	FM	KING A.	5 First Street,	Booragul	2284	
N	FM	KING G.	5 First Street,	Booragul	2284	
HO	SM	KIRKLAND D.	47 Napoleon Street,	Sans Souci	2219	529 8612
N	OM	KLIMPSCH G.N.	69 Annesley Street,	Leichhardt	2040	569 4017
N	FM	KNIFE Mrs K.	6/1 Thorsby Way,	Ambarvale	2560	04626 5665
EM(UK)	FM	KNIFE P.A.	P.O. Box 797,	Campbelltown	2560	04626 5665
N	OM	KYLE J.	99 Douglas Street,	Stockton	2295	28 1066
N	OM	LAMBERT R.H.Y.	3 Daisy Street,	Chatswood	2067	411 2723
HO	FM	LARMOUR G.	42 McMillan Street,	Yagoona	2199	705 5074
HO	FM	LARMOUR Mrs J.	42 McMillan Street,	Yagoona		" "
HO	OM	LARMOUR M.	42 McMillan Street,	Yagoona	2199	" "
HO	OM	LARMOUR P.	189 Rex Road,	Georges Hall	2198	727 0458
HO	OM	LASZLO I.M.	2 Spowers Circuit,	Holder	2611	06288 3736
HO	OM	LAURIE E.R.	10/684 Rocky Point Road,	Sans Souci	2219	
O-USA	FM	LEE J.	3/153 Woniara Road,	Hurstville South	2221	546 2249
O-USA	FM	LEE Mrs J.	3/153 Woniara Road,	Hurstville South	2221	546 2249
HO	OM	LEE P.J.	79 Terrigal Street,	Marayong	2148	671 3712
HO	OM	LINDEMAN J.P.	5 Hezlet Street,	Chiswick	2046	712 1162
HO/OO	PLM	LONES Dr R.I.	P.O. Box 252,	Manuka	2603	
O	OM	LOTHIAN S.A.	92 Parthenia Street,	Dolans Bay	2229	525 2318
HO/O	OM	LOVETT B.E.	26 Blaxland Road,	West Killara	2071	498 4659
HO	OM	LOW D.H.	8 Hendy Avenue,	Panania	2213	774 1140
HO	OM	LOWRY T.	23 Hampton Street,	Croydon Park	2133	798 5578
HO	OM	MARTIN J.	233 Avoca Drive,	Green Point	2250	04369 5094
HO	OM	LYNESS K.	628 Princes Highway,	Kirrawee	2232	517 1663
HO	OM	MARION D.E.	18 Hall Pde.,	Hazelbrook	2779	04758 6601
HO(nsw)	OM	MAY T.H.	21 Hawke Street,	Blayney	2799	06368 2336
O	PLM	MCDONAGH D.R.	141 Barber Street,	Gunnedah	2380	06742 0555
HO/OO	OM	MCINERNEY J.	14 Angelo Street,	Burwood	2134	747 5894
HO/OO	OM	MCLEOD C.R.	P.O. Box 42,	Holbrook	2644	06036 2333
	OM	MCMAHON P.J.	10/6 Burraneer Bay Road,	Cronulla	2230	523 4137
OO	OM	MEREDITH D.	2b Percival Street,	Penshurst	2222	579 3791
N	PLM	MIDGLEY I.	19 Derain Crescent,	Hazelbrook	2779	
HO	OM	MILLER M.J.	95 Jubilee Street,	Dubbo	2830	06882 5176
HO/OO	OM	MOORE G.W.C.	23 Murrallah Place,	Lane Cove	2066	428 5680
HO	OM	MOORE T.	5 Callistemon Close	Epping	2121	86 3522
HO	OM	MORCOM P.J.	4/11 Pitt Street,	Mortdale	2223	57 7520
HO	OM	MORGAN P.	39 Renway Avenue,	Lugarno	2210	533 4045
O(NSW)	OM	MORRIS R.	39 Greene Avenue,	Ryde	2112	808 2380
HO	OM	NIGHTINGALE C.T.	85 Connell's Point Road,	South Hurstville	2221	546 2433
HO/N	OM	MCRTON B.	P.O. Box 87,	Granville	2142	799 7924
HO	OM	O'BRIEN D.	3 Marion Street.	Seven Hills	2147	621 2626



HO	OM	O'CONNOR R.I.	13 Kisdon Crescent,	Prospect	2149	621 5005
HO	OM	OGSTON R.A.	505 Yarra Place,	St Johns Park	2176	610 2826
HO	OM	OLDFIELD K.A.	13 Elvidge Crescent,	Kotara South	2288	04943 2588
HO/N	OM	OPREY W., Hacienda C/Crt	Cooperwall Road,	Lithgow	2790	
HO	OM	OTTEY W.	72 Merrigang Street,	Rowral	2576	04861 3384
HO/Sn31/2	OM	PARKER J.	34 Strickland Street,	Bass Hill	2197	724 5348
HO	OM	PARKES T.E.	85 Soldiers Road,	Jannali	2226	528 9047
HO	OM	PARSONS R.A.	Suite 4, Nelson Bay Centre,	Nelson Bay	2315	
HO	OM	PECK F.	33 Henson Street,	Merrylands	2160	637 9896
O(NSW)	OM	PERRY P.	146 Denison Street,	Bondi Junction	2022	389 5436
N	OM	PHILPOTT S.L.	7 Koorang Place,	Bangor	2234	543 4461
HO	OM	POTTS F.R.	23 Frampton Avenue,	Marrickville	2204	560 8070
HO	SM	POWELL M.	30 Morris Avenue,	Kingsgrove	2208	
HO n2 1/2	OM	PRESTON A.L.	1064 Old Princes Highway	Engadine	2233	520 8590
HO	OM	PRIESTLEY R.D.	150 The Kingsway,	Cronulla	2230	523 2909
OO	OM	RANSAY N.W.	4 Waldron Street,	Sans Souci	2219	529 8698
O	HLM	READ N.	3 Augusta Street,	Strathfield	2135	642 3828
P4	OM	REID J.R.	2/16 Croydon Street,	Petersham	2049	569 2970
HO	OM	RICHARDSON K.	28 Caloola Crescent,	Beverly Hills	2209	50 6923
HO	OM	ROBINSON F.	Norwood, Lot 26 Chifley Road,	Dargan,	2786	06355 2611
HO	OM	ROBINSON N.	Lot 1196 Cabernet Avenue,	Eaglevale	2558	603 1221
HO/OO	OM	RODGERS B.A.	1 Elanora Close,	Baulkham Hills	2153	639 8548
O	OM	ROWLING B.	42 Pemberton Street,	Parramatta	2150	630 1475
HO	OM	RYAN L.J.	16 Shelley Street,	Cowra	2794	06342 1906
HO(AUST)	OM	RYLANCE W.J.	10 Queen Street,	Marrickville	2204	558 5394
HO/SN31/2	OM	SANDERSON P.	8 Skinner Street,	Ballina	2478	06686 2708
O	OM	SHEPHERD C.	14 Beauchamp Street,	Marrickville	2204	55 7846
OO	FM	SHIRLEY W.A.	62 Allchin Circuit,	Kambah	2902	06247 4611
HO	FM	SHOEBRIDGE H.J.	24 Brisbane Street,	Chifley	2036	
HO	FM	SHOEBRIDGE G.J.	24 Brisbane Street,	Chifley	2036	
	FM	SHOEBRIDGE P.F.	24 Brisbane Street,	Chifley	2036	
HO	OM	SISSON D.E.	12 Viburnum Road,	Loftus	2232	521 3446
HO	OM	SKILTON J.	41 Juvenis Avenue,	Oyster Bay	2225	528 9316
HO/N	OM	SMITH B.	2/58 Barry Place,	Bidwill	2770	628 5361
HO/O	OM	SORENSEN S.N.	143 Kiara Road,	Miranda	2228	525 4996
ALL	OM	SOUTAR G.M.	Box 544 P.O.,	Sutherland	2232	542 1669
OO	OM	SPEECHLEY P.L.	62 Austin Street,	Illawong	2234	
HON3	OM	SPURWAY D.J.	16 Thorntons Road,	Queanbeyan	2620	06297 3553
HO	OM	STAINLAY G.T.	P.O. Box 292,	Murwillumbah	2484	06672 3221
HO	OM	STEBBING J.	57 Linden Street,	Sutherland	2232	521 7457
HO	FM	STEELE C.	11 Magnolia Street,	St Marys	2760	623 4570
HO	FM	STEELE Mrs R.	11 Magnolia Street,	St Marys	" "	" "
O	OM	STELL F.	14 Whitegates Avenue,	Peakhurst	2210	534 5121
HO	OM	STELL R.A.	17 Beach Street,	Kogarah	2217	587 6381
O/HO/1*	OM	STEPHAN A.D.	"The Lodge", P.O. Box 169,	North Ryde	2113	887 1276
HO	OM	STEVENS G.	68 Shallow Crescent,	St Clair	2759	670 3219
HO	FM	STEVENSON Mrs V.	86 Merton Street,	Sutherland	2232	
HO/OO	FM	STEVENSON R.V.	86 Merton Street,	Sutherland	2232	521 1196
	OM	TAYLOR B.	4 School Parade	Westmead	2145	
HO	OM	TEMPLEMAN A.J.	5 Kenilworth Road,	Lindfield	2070	46 1176
HO/OO	OM	THOMAS H.N.	147 First Avenue,	Sawtell	2452	06653 2764
HO	OM	THOMPSON F.L.	24 Fearn Street,	Taongabbie	2146	636 3378
HO	OM	TOLLEY R.J.	57 Greiville Crescent,	Macquarie Fields	2564	
O & Z	OM	TONURIST E.A.	P.O. Box 417,	Penrith	2750	04733 1832
N	SM	TURNER D.	275 Forest Road,	Kirrawee	2232	521 2882
HO/O	HM	TYSON B.K.	1/17 Norman Street,	Concord	2137	73 5086
	FM	TYSON D.A.	1/17 Norman Street,	Concord	2137	73 5086
HO	OM	WALLACE M.	7 Cutler Parade,	North Ryde	2113	88 4089
HO	OM	WALMSLEY R.H.	111 Attunga Road,	Yowie Bay	2228	525 1887
HO	FM	WARDROP R.G.	25 Manchester Road,	Gymea	2227	524 5376
HO	FM	WARDROP Mrs D.	25 Manchester Road,	Gymea	" "	" "
HO(AUST)	OM	WARREN C.S.	28 Durbar Avenue,	Kirrawee	2232	521 1189
HO(AUST)	OM	WARREN H.S.	28 Durbar Avenue,	Kirrawee	2232	" "
HO	FM	WATSON B.W.	7 Glenview Place,	Engadine	2233	520 2264
HO	FM	WATSON K.J.	7 Glenview Place,	Engadine	2233	
O	OM	WEATHERS C.	58 Taylor Street,	Annandale	2038	660 5775
HO/HON31/2	OM	WEBB Smn. ATA-S139874	RAN, Air Station,	Nowra	2540	
HO	OM	WEBB W.J.	P.O. Box 68,	Figtree	2525	04229 5824
HO(Aust)	OM	WEEKS R.J.	38 Bridge Street,	Cessnock	2325	04990 1716
HO/O	OM	WILKINSON N.P.	1 Tamah Street,	Epping	2121	
HO	OM	WILKINSON W.L.	4 Purkiss Street,	Armidale	2350	06772 3892
HO/Z	OM	WILSON J.R.	29 Winnicoopa Road,	Blaxland	2774	04739 4777
HO/OO	OM	WITHERS D.R.	213 Galston Road,	Hornsby Heights	2077	477 4622
HO/N	OM	WHALE W.	65 Kitchener Avenue,	Earlwood	2206	
HO	OM	WYATT J.E.	3/18-20 Jessie Street	Westmead	2145	635 3301
HO	PLM	WILCOX K.J.	23 Clermont Avenue,	North Strathfield	2137	73 2232
HO	SM	XUEREB D.	19 Woodstock Street,	Botany	2019	666 4004
HO	OM	YOUNG A.P.	35 Clearview Parade,	Hazelbrook	2779	58 6777
HO/N	OM	ZLATAROFF B.	4/370 Bay Street,	Brighton Le Sands	2216	597 1972



## AUSTRALIAN MODEL RAILWAY ASSOCIATION

## VICTORIAN BRANCH- MEMBERSHIP LISTING 1 NOVEMBER, 1986.

		AMRA VIC BRANCH			
HO	OM	AA L.	68 Lahona Avenue,	Bentleigh East	3165
OO	OM	ANDREW J.K.	6/23 Coorigil Road,	Carnegie	3163 568 4018
OO	OM	ATRILL G.H.	6 Coolangatta Terrace,	Dromana	3936 05987 2813
HO	HLM	BAKER M.	38 Frederic Drive,	Ringwood	3134 876 2507
HO/HON21/2	OM	BARTLETT W.N.	28 Ligar Street,	Sunbury	3429 744 3536
Z	FM	BLANKFIELD A.	12 First Street,	Footscray West	3012 68 5649
Z	FM	BLANKFIELD P.	18 Hill Street,	Toorak	3142 241 4929
HO/	OM	BLACKLEY R.J.	18 Hill Street,	Toorak	3142 " "
HO/HON21/2	OM	BOGIE R.	4 Fernwood Road,	Fountain Gate	3805 704 7629
HO(Vic.ra.)	OM	BRISBANE W.D.	Box 5439cc, G.P.O.,	Melbourne	3001 560 5827
HO	OM	BROOK A.J.	2 Moray Street,	East Bentleigh	3165 570 7886
HO	OM	BROOKS G.	42 Huddle Street,	Essendon	3040 370 8268
HO/OO	OM	BROUGH J.C.	337 Elgar Road,	Surry Hills	3127 890 0747
HO/OO	OM	BROWN A.J.	158 Balwyn Road,	Balwyn	3103 857 9106
HO	OM	BROWN D.J.	1 Gloucester Place,	Warragul	3820 05623 2294
HO	FM	BROWN G.H.	13 Prospect Street,	Mount Waverley	3149 288 6866
HO	FM	BROWN Mrs S.	3 Rivette Court,	Keilor Downs	3038 367 6203
HO	OM	BURMAN P.J.	3 Rivette Court,	Keilor Downs	3038
1:50	OM	BURROWS L.	27 Rowitta Drive,	Glen Waverley	3150 232 0757
HO	OM	CANDY G.W.	Box 1937R, G.P.O.,	Melbourne	3001 819 1635
HO	OM	COATES J.D.	23 Longbourne Avenue,	Notting Hill	3168 560 8936
HO(Vic)	OM	COLGAN A.J.	29 Bright Crescent,	Mt. Eliza	3930 787 4642
N	OM	COLLIVER J.	21 Kerana Avenue,	Reservoir	3073 460 2086
O	OM	COMFORT B.R.S.	18 Reumah Court,	Heathmont	3135 870 3027
O	SM	COMFORT I.	P.O. Box 100,	North Brighton	3186 596 3249
OO/N-V/LINE	OM	COUACAUD P.	P.O. Box 100,	North Brighton	3186
HO	FM	COWARD G.F.	P.O. Box 27, R.A.A.F Base,	East Sale	3852 05149 4294
HO	FM	COWARD D.G.	8 Ruth Street,	Donvale	3111 03842 4619
OO	FM	CROFT H.R.	8 Ruth Street,	Donvale	3111
OO	FM	CROFT Mrs F.L.	29 Murdo Road,	Clayton	3168 543 5897
HO	OM	CROPLEY R.M.	29 Munda Road,	Clayton	3168
HO/OO	OM	CROSS F.J.	27 Rosebank Avenue,	North Ringwood	3134
HON21/2	GM	CROYDON NARROW GAUGE GRP	25 Harwell Road,	Fern Tree Gully	3156 758 3969
HO(D&RGW)	OM	DALL R.C.W.	P.O. Box 155,	Croydon	3136 723 1211
HO	HLM	DEAN E.W.R.	7 Murchison Avenue,	Vermont South	3133 221 6034
HO	HLM	DEAN Mrs F.	105 Yarralea Street,	Alphington	3078
HO(U.S.A.)	OM	DERRICK C.W.	105 Yarralea Street,	Alphington	3078
HO/HON3	OM	DORAN T.N.	1/128 Kingston Road,	Heatherton	3202 551 4780
OO	OM	DOWEL A.N.	C/- Buffer Stop, 535 Plenty Rd,	Preston East	3072 478 0038
HO	OM	DOWN K.N.	82 East Boundary Road,	East Bentleigh	3165 579 4343
HO/OO	HLM	DUNLOP Mrs M.	3/2 Wattle Avenue,	Glenhuntley	3163 578 9808
HO/OO	HLM	DUNLOP T.	103 Yarralea Street,	Alphington	3078
HO(VR)	OM	DUNN R.P.	103 Yarralea Street,	Alphington	3078 497 3093
OO(BR)	FM	EBINGER M.	10 Holland Road,	Blackburn South	3130 878 4914
OO(BR)	FM	EBINGER Mrs C.	21 Whitton Grove,	Werribee	3030 741 3267
HO	OM	EDWARDS R.H.	21 Whitton Grove,	Werribee	3030 741 3267
HO	OM	EDWARDS R.J.	21 Robinlee Avenue,	East Burwood	3151 232 6887
HO	OM	EDWARDS R.D.	21 Robinlee Avenue,	East Burwood	3151
OO	OM	ELLIOTT G.	21 Robinlee Avenue,	East Burwood	3151
HO	OM	ELY O.S.	P.O. Box 228,	Berwick	3806 707 1810
HO	OM	EMMETT J. (Jnr.)	10 Koonalda Avenue,	Glen Waverley	3150 561 1941
OO	OM	ENGLAND P.J.	P.O. Box 299,	Mentone	3194 580 5464
OO	OM	FAIRHURST L.	5/57 Royal Avenue,	Sandringham	3191 598 8665
HO	OM	FAULKNER L.J.	363 Diamond Creek Road,	Diamond Creek	3089 438 1523
HO	OM	FISHLEY K.R.	220 Russell Street,	Dennigton	3280 05562 8674
O	FM	FLETCHER C.E.F.	P.O. Box 89,	Glen Waverley	3150 560 9325
O	SM	FLETCHER D.	30 Roseland Grove,	Doncaster	3108 848 7256
O	FM	FLETCHER J.	30 Roseland Grove,	Doncaster	3108
O	FM	FLETCHER M.	30 Roseland Grove,	Doncaster	3108
O	FM	FLETCHER E.	30 Roseland Grove,	Doncaster	3108
HO	OM	FORMAINI M.	171 Blackburn Road,	Mount Waverley	3149 233 3085
OO	OM	FOX G.	2/65 Royal Avenue,	Sandringham	3191 598 9321
HO/HON3	OM	FRASER G.J.	9 Penrith Court,	Eltham	3095 439 1572
OO	OM	FRARY B.D.	21 Monkhouse Drive,	Endeavour	3802 560 7133
N	OM	FROST J.	8 Tristania Street,	Doncaster East	3109 848 2236
HO	PLM	GARDNER J.R.	3 Ramu Grove,	Ashburton	3147 25 7221
HO	SM	GERRING N.A.	P.O. Box 84,	Surry Hills	3127 898 7088
HO/OO/N-ALL	FM	GIBSON J.R.	C/- P.O. Box 685 ,	Traralgon	3844 05174 3468
HO/OO/N	FM	GIBSON Mrs E.	C/- P.O. Box 685,	Traralgon	3844
HO	OM	GIBSON N.W.	8 Clark Street,	Reservoir	3073 470 2764
N	OM	GILLSON C.R.	18/9 Canterbury Road,	Toorak	3142 241 7306
HO/N	OM	GOVETT M.J.	RMB 2028,	Violet Town	3669
N	OM	GUILLARD K.H.	Cnr. High and Sherry Sts.,	Chiltern	3683
O/On21/2	OM	GUTTERIDGE Dr R.B.	50 Belmore Street,	Yarrowonga	3730 05744 3120
OO	OM	HALL J.	3 Michael Street,	Scoresby	3179 763 7143
HO/N	OM	HANCOCK W.C.	1 Penleigh Court,	Mount Waverley	3149 277 6541
HO	OM	HARDING G.P.	11 Stirling Road,	Metung	3904 05156 2369
HO (Aust)	OM	HARRY J.J.	68 Lahona Avenue,	Bentleigh East	3165 570 4406
N	OM	HARVEY D.S.	46 Somerville Lane,	Riddells Creek	3431 05428 6476
O/OO	OM	HAUSTORFER M.	18 Ibbottson Street,	Watsonia	3087 435 8449
HO/OO	PLM	HAWKEN G.M.	2 Boston Road,	Bundoora Heights	3083 435 3725



OO/HO	OM	HAYES R.D.	"Ipwinger", Lanning Crescent,	Seville	3139	05964	4931
HO	FM	HEALE R.	17 Dorrington Avenue,	Glen Iris	3146	25	7856
HO	FM	HEALE A.	17 Dorrington Avenue,	Glen Iris	3146		
HO/HO21/2	OM	HILLEBRAND P.	71 Oransay Crescent,	Diamond Creek	3089	438	3301
HO21/2	OM	HOAD A.	13 Kia-ora Parade,	Ferntree Gully	3156	758	6498
HO	OM	HOARE W.R.	16 Druid Street,	Golden Square	3555	05447	8389
HO	FM	HOCKING B.	143 First Avenue,	Rosebud	3939	05986	7107
	FM	HOCKING Mrs P.	143 First Avenue,	Rosebud	3939		
HO	OM	HUMPHRIES J.A.	38 Highridge Crescent,	Airport West	3042	338	0938
HO/DO	OM	HUNTER L.	4 Clarendon Court,	Mooroolbark	3138	726	9779
HO	OM	HURLEY K.B.	43 Fakenham Road,	Ashburton	3147	25	5349
N	OM	INCHLEY E.A.	6 First Avenue,	North Dandenong	3175	795	3947
HO/N/Z(Eur)	OM	JACKA Mrs M.A.	893 High Street Road,	Glen Waverley	3150	233	3067
OO/HO	FM	JAMES R.D.	2/37-38 Railway Pde,	Murrumbene	3163	568	3607
	FM	JAMES Mrs C.	2/37-38 Railway Pde,	Murrumbene	3163		
OO	OM	JENKINSON A.W.	7 Frawley Street,	Frankston	3199	783	4284
OO	FM	JENKINSON Mrs C.	Latrobe Road,	Yarrambat	3091	436	1089
HO	FM	JENKINSON P.A.	Latrobe Road,	Yarrambat	3091	436	1089
HO	OM	JOHNSON A.F.H.	22 Leicester Avenue,	Glen Waverly	3150	233	1118
HO/HO21/2	OM	JOHNSON M.R.	12/80 Wellington Road,	Clayton	3168	543	3261
	OM	KERR J.H.	6 Rees Court,	Mount waverley	3149	544	4696
O	OM	KNIGHT J.L.	72 Strathallan Road,	Macleod	3085	45	2531
OO	OM	KOHEK F.	9 Eddie Street,	St. Albans East	3021	366	7527
HO(VR)	OM	LAIDLAY M.A.	"Hazelale", Chapel Lane,	Nutfield	3099	714	8244
HO	OM	LANGHAM W.O.	93 Glyndon Road,	Camberwell	3124	29	6374
OO	OM	LAWSON C.E.	12 Faraday Street,	Boronia	3155	762	4018
HO	OM	LEES D.W.	507 The Boulevarde,	East Ivanhoe	3079	49	6308
HO	HLM	LITTLE T.R.	P.O. Box 46,	Nunawading	3131		
HO	OM	LLEWELYN A.L.	6/5-7 Porter Road,	Heidelberg Heights	3081	458	3643
HO	FM	LLOYD M.G.	6 Kiers Court,	Rosanna East	3084		
HO	HON	LLOYD R.C.	6 Kiers Court,	Rosanna East	3084	459	6508
HO	OM	LORD K.F.	RMB 1175, Glenmaggie Road,	Heyfield	3858	05148	2212
HO	OM	MACDONALD J.D.R.	28 Grandview Road,	Brighton	3186	596	4221
HO	OM	MADDICKS A.G.	4 Brownlee Crescent,	Mulgrave	3170	560	1918
HO/DO	OM	MAGUIRE I.C.	P.O. Box 203,	Ashburton	3147	25	3454
HO/DO	SM	MAGUIRE M.I.	P.O. Box 203,	Ashburton	3147	25	3454
HO/DO	SM	MAIN R.K.	8 Kalang Road,	Camberwell	3124	29	7662
HO	FM	MALONEY L.J.	23 Kingston Street,	Hampton	3188	598	6791
	FM	MALONEY R.	23 Kingston Street,	Hampton	3188		
HO	OM	MALONEY M.V.	16 Warrata Avenue,	Delacombe	3356	05335	6466
HO	FM	MARSDEN R.J.	21 Lerina Street,	Clayton	3168	544	5079
HO	FM	MARSDEN S.C.	21 Lerina Street,	Clayton	3168		
HO	FM	MARSDEN D.A.	21 Lerina Street,	Clayton	3168		
HO	FM	MARSDEN J.A.	21 Lerina Street,	Clayton	3168		
OO(BR)	OM	MARTIN M.B.	32 Gnarwyn Road,	Carnegie	3163	67	5455
HO	OM	MARTIN T.J.	"ARAWA" Church Street,	Fryerstown	3451		
HO	OM	MATTHEWS I.	14 Keith Grove,	Ringwood	3134	870	8975
	OM	MCCALLUM J.D.	95 Gowrie Street,	Glenroy	3046		
HO	OM	MCCLURE J.A.	19 Brynmawr Road,	Camberwell	3124	299	2419
HO/DO	OM	MCINTOSH I.G.	15 Ferguson Street,	Macleod	3085	459	2613
HO	OM	MCKENNA A.	7 Max Court,	Noble Park	3174		
HO	HLM	MCKINNON M.	50 Rutley Crescent,	Echuca	3564		
HO	OM	MEHLMAN D.I.	16/66-70 Grosvenor Street,	East St. Kilda	3182		
HON3/N	OM	MELDRUM D.	23 Beverley Street,	Doncaster East	3109	842	3034
HO	OM	MILLER A.	905 Dandenong Road,	East Malvern	3145	211	5297
HO/DO	OM	MITCHELL K.J.	9 McMillan Grove,	Paynesville	3880	05156	6703
HO	OM	MONISH R.J.	2 Byrne Court,	Cheltenham	3192	583	7033
HO	OM	MOREHOUSE W.J.	6 Outlook Road,	Mount Waverley	3149	277	1717
HO	OM	MORRISON D.J.	192 Patterson Road,	Moorabbin	3189	557	2382
HO	OM	MUNSON S.	P.O. Box 419,	Malvern	3144	20	6529
OO	OM	MURLIS N.E.	34 Centre Road,	Vermont	3133	874	2860
O	OM	NICOLSON R.F.L.	8 Birch Street,	Mentone	3194	584	7131
HO(VR)	OM	NIMON D.	18 Keyes Street,	Ashburton	3147	25	4819
HO/O.VR-USA	OM	NITZ G.	20 Alpha Street,	North Balwyn	3104	857	6959
O-USA	FM	NIVEN J.G.	'Kiam' Macnamara Street,	Ferny Creek	3786	755	1333
O-USA	FM	NIVEN G.	'Kiam' Macnamara Street,	Ferny Creek	3786		
O-USA	FM	NIVEN D.L.	'Kiam' Macnamara Street,	Ferny Creek	3786		
HO	FM	NORMAN D.J.	33 Bruce Street,	Mt. Waverley	3149	03277	7949
HO/DO/N	OM	NOSKE R.E.	58 Fairview Avenue,	North Croydon	3136	725	7693
HO	OM	O'FLYNN G.H.	6 Laurie Court,	Warrnambool	3280	05562	6704
HO/HO21/2	OM	PERRIN D.T.	51 Bryson Grove,	Lower Templestowe	3107	850	4223
HO/HO21/2	OM	PETRIE S.D.C.	36 Bateman Street,	Hampton	3188	597	0045
	OM	POLISTENA R.J.	1 Loxwood Avenue,	Keysborough	3173	798	7609
HO/N	OM	POLLARD R.J.	28 Teal Crescent,	Lalor	3075	465	6176
HO21/2	OM	PREWETT R.E.J.	8 Moore Street,	Bacchus Marsh	3340	05367	1503
HO	SM	PRICE D.R.	P.O. Box 17,	Huntly	3551	05448	8428
HO/DO	OM	RICHARDS D.L.	7/36 Nepean Avenue,	Moorabbin	3189	555	9059
ON21/2	HLM	RICHARDSON C.E.	18 Lesley Street,	Burwood	3125	29	5899
HO/HO21/2	HLM	ROLFE C.C.	Post Office Box 24,	Loch	3945	05659	4365
HO	OM	ROWBURY H.T.	1a Carool Road,	Murrumbene	3163	569	5946
ON21/2	OM	RICHARDSON G.	1/3 Suffolk Road,	Surrey Hills	3104	859	9316
HO(FR)	OM	RADDATZ E.F.	P.O. Box 201,	St. Albans	3021	366	2973
OO/N	OM	RICHES N.R.	5 Amber Court,	Cheltenham	3192	583	1587



HO/HOn3	OM	RICHIE P.E.	28 Ascot Street South,	Ballarat	3350	05332 1138
OO	OM	RIGBY P.J.	84 Hansworth Street,	Mulgrave	3170	547 8627
HO/OD	SM	ROLLS R.J.S.	17 Macfarlan Street,	South Yarra	3141	267 1592
HO	FM	ROSS D.A.	67 Warburton Highway, Wesburn,	Via Yarra Junction	3977	05967 1581
HO	FM	ROSS L.	67 Warburton Highway, Wesburn,	Via Yarra Junction	3977	
HO	FM	ROSS G.	67 Warburton Highway, Wesburn,	Via Yarra Junction	3977	
OO/HO	OM	ROTHERY E.	7/62 Serrell Street,	East Malvern	3145	572 1523
HO/OD	OM	ROWLAND G.	21 Purinuan Road,	Reservoir	3073	460 2302
OO	OM	ROYSTON W.S.	21 Ardmillan Road,	Moonee Ponds	3039	370 7590
HO	OM	RYAN G.	6 Toora Street,	Ivanhoe	3079	49 6374
HO/N	OM	SCARFF B.	5 Reservoir Road ,	Newlyn	3364	
HO/OD	OM	SCOTT W.M.	P.O. Box 116,	Hastings	3915	05983 9912
HO	FM	SECKER W.M.	2 Hilltop Avenue,	Glen Iris	3146	25 8360
HON	FM	SECKER Mrs E.	2 Hilltop Avenue,	Glen Iris	3146	
HO	FM	SCANTLEBURY N.H.	239 Whitehorse Road,	Balwyn	3103	817 2238
HO	FM	SCANTLEBURY C.	239 Whitehorse Road,	Balwyn	3103	
HO	FM	SCANTLEBURY F.	239 Whitehorse Road,	Balwyn	3103	
HO	FM	SCANTLEBURY G.	239 Whitehorse Road,	Balwyn	3103	
HO/HOn21/2	FM	SCHACHE G.J.	1 Redmond Court,	Mount Waverley	3149	232 0863
HO/HOn21/2	FM	SCHACHE A.J.	1 Redmond Court,	Mount Waverley	3149	
HO/HOn21/2	FM	SCHACHE P.J.	1 Redmond Court,	Mount Waverley	3149	
HO/N	OM	SCHENDZIELORZ H.	10 Harvey Place,	Endeavour Hills	3802	700 1770
HO	OM	SELL A.E.	P.O. Box 315,	Box Hill	3128	299 2178
OO/O	OM	SHEERAN F.	5 Hume Court,	Ashwood	3147	277 0827
OO	OM	SHEPPEE D.A.	10 Winston Street,	Glen Waverley	3150	233 1771
OO	OM	SHERLOCK L.T.	22 Mora Avenue,	Oakleigh	3166	569 8776
O	OM	SHERLOCK R.A.	Lot 12 Bryants Road,	Linton	3360	05344 7359
HO/OD(VR)	OM	SMITH J.	59 Smythe Street,	Corinella	3984	05678 0453
HO	OM	SMITH R.	5 Dowle Street,	Macleod	3085	435 3957
N	OM	SNELL E.C.	43 Scott Street,	Vermont	3133	
HO	OM	SOUTHWELL T.B.	1 Wellington Avenue,	Blackburn	3130	878 9002
HO	OM	STILL G.	10 Cornwall Crescent,	Mount Martha	3934	05974 2422
OO	FM	STOCKFELD G.	14/25 Ashted Road,	Box Hill	3128	898 6806
OO	FM	STOCKFELD Mrs J.	14/25 Ashted Road,	Box Hill	3128	
N	OM	STRATFORD L.J.	9 Van Diemen Avenue,	Nunawading	3131	877 2558
O/OD	OM	STUCHBERY W.A.	Latrobe Road,	Yarrambat	3091	436 1112
O/OD	GM	SUNBURY MODEL RAILW CLUB	P.O. Box 437,	Sunbury	3429	
HO	OM	SWALE J.	9 Larbert Avenue,	North Balwyn	3104	857 5567
HO/OD	OM	THOMAS J.H.	3 Maple Court,	Wodonga	3690	06024 3675
HO(VR)	OM	THIELE I.O.	135 Huntingdale Road,	Oakleigh	3166	544 6602
N	OM	THOMAS R.	3 Sturdee Road,	Black Rock	3193	598 5858
HO/OD	OM	THORPE N.F.	511 The Boulevard,	East Ivanhoe	3079	49 4525
HO/OD	HLN	TRESEDER J.J.	55 Creek Road,	Mitcham	3132	874 3953
HO	OM	WATKINS D.	10 Henderson Road,	Tullamarine	3043	
HO	OM	WATSON E.G.	18 Glencannon Crescent,	Clayton	3168	
N	SM	WEIDEMANN D.C.	6 Batman Grove,	Mulgrave	3170	560 2267
OO	HLN	WESTERMAN S.	10 Gardenia Crescent,	Cheltenham	3192	583 8655
HO/OD(BR)	SM	WESTERMAN B.L.	10 Gardenia Crescent,	Cheltenham	3192	
HO/N	FM	WHITE C.C.	7 Riverview Crescent,	Doveton	3177	793 2280
	FM	WHITE M.D.	7 Riverview Crescent,	Doveton	3177	
HO	OM	WIERZBINSKI M.	468 Warrigal Road,	Ashburton	3147	25 4478
HO	OM	WRIGGLESWORTH R.E.	2 Terrigal Court,	Bayswater	3153	729 9492
HO	OM	WILKE H.H.	1 Waterloo Street,	Camberwell	3124	882 7046
HO	OM	WILLIAMS Dr A.T.	141 Maling Road,	Canterbury	3126	836 4342
Z/N/HO	OM	WILLMOTT G.T.	297 Clarendon Street,	Thornbury	3071	480 1310
N/ HO	SM	WILSON I.	R.M.B.2454, Flowerdale,	Via Broadford	3658	05780 1434
O/OD	OM	WIRTH J.	11 Gums Avenue,	Tecoma	3160	754 4580
HO	SM	WOOD S.M.	53 Fordham Road,	Reservoir	3073	460 1281
HO(LMS)	OM	WOODS A.	10 Monara Close,	Wantirna South	3152	221 7860
HO(VR)	FM	YOUNG R.T.	109 Elder Street,	Greensborough	3088	434 2631
HO	FM	YOUNG D.S.	109 Elder Street,	Greensborough	3088	

## AUSTRALIAN MODEL RAILWAY ASSOCIATION

## QUEENSLAND BRANCH- MEMBERSHIP LISTING 1 NOVEMBER, 1986.

		AMRA QLD. BRANCH	32 Wana Street,	Sunnybank	4109	
HO	OM	ANDERSEN A.K.	40 Sloane Street,	Stafford Heights	4053	359 8525
HO	OM	ARMSTRONG J.	27 Richardson Street,	Chelmer	4068	379 7687
OO/N	OM	BALCOMBE-JESTICO E.	16 Parnell Street,	Ormiston.	4163	286 2639
HO	OM	BATCH A.E.	16 Railway Street,	Buranda.	4102	391 3032
HO	OM	BERRY S.H.G.	28 Yinni Street,	Maraachydore.	4558	07143 3232
Sn31/2	OM	BILBY J.G.	68 Mashobra Street,	Mitchelton.	4053	355 9021
HO/Sn31/2	OM	BLAKELEY D.I.	13 Aberfeldy Street,	Kenmore	4069	378 2741
HO	FM	BOWRING G.J.	P.O. Box 183,	Upper Mt. Gravatt	4122	
HO	FM	BOWRING Mrs J.J.	P.O. Box 183,	Upper Mt. Gravatt	4122	
HO/OD	OM	BRABAZON M.	2/48 Bristol Street,	Westend	4101	844 9069
N	OM	CALCRAFT A.	21 Lucy Street,	Milton	4064	369 8884
	GM	CALOUNDRA & DIST.RAIL.ASS	52 Albatross Avenue,	Caloundra	4551	91 2660
HO	OM	CARLSSON D.	P.O. Box 217,	Zillmere	4034	07269 8127
HO/HOn	OM	CASH G.A.	117 Alice Street,	Goodna.	4300	
HO	OM	CHRISTIE J.R.	32 Wana Street,	Sunnybank.	4109	345 4071
HO	OM	CLELLAND R.A.	11 Sunnyview Street,	Beenleigh	4207	
HO/TT	OM	COOMER G.	4 Ronald Street,	Bray Park	4500	205 4018
HO/OD	OM	CORCUT J.	P.O. Box 177,	Warwick	4370	07661 4142
HO	OM	COWEN K.J.	3 Wandearah Crescent,	Ferny Hills	4055	351 5018



HO	FM	COKLEY K.V.	P.O. Box 813,	Burleigh Heads	4220	
HO	FM	COKLEY P.	P.O. Box 813,	Burleigh Heads	4220	
HO	SM	COSTIN B.McA.	28 Kildare Street,	Carina Heights	4152	07398 8701
HO	OM	COX I.C.	3 Jura Street,	Middle Park	4074	376 4409
HO	OM	DALY B.A.	241 Bapaume Road,	Holland Park West	4121	349 8415
Sn31/2	FM	EDGE-WILLIAMS K.	P.O. Box 38,	Lowood	4311	07586 1186
	FM	EDGE-WILLIAMS Mrs W.E.	P.O. Box 38,	Lowood	4311	
HO n3	OM	FAINGES J.	60 Fallon Street,	Everton Park	4053	355 4408
HO/HOn31/2	OM	FORD P.	P.O. Box 754,	Mackay	4740	0757 2475
HO n31/2	OM	GEDDES W.	30 Rossett Street,	West Chermide	4032	266 3098
HO	OM	GILBY L.R.	37 Granville Street,	Biloela	4715	
HO/N	OM	HARLAND A.H.	P.O. Box 1577,	Southport	4215	07532 1668
HO/N	HLM	HARROLD A.	20 Allambi Rise,,	Noosa Heads.	4567	07147 3232
HO n31/2	FM	HAYES W.A.	18 Joyce Street,	Coopers Plains.	4108	07345 7887
HO	FM	HAYES K.D.M.	18 Joyce Street,	Coopers Plains	4108	
O	OM	HIGGINS A.W.	14 Hiron Street,	St Lucia.	4067	370 7802
HO n	FM	HILL J.A.	11 Freesia Street,	MacGregor.	4109	343 6078
HO-NSW	FM	HILL Mrs M.L.	11 Freesia Street,	MacGregor.	4109	
SN31/2	FM	HILL M.	11 Freesia Street,	MacGregor.	4109	
SN31/2	FM	HILL S.A.	11 Freesia Street,	MacGregor.	4109	
HO	OM	HOSKINS J.L.	3 Drake Road,	Springwood	4127	208 5288
HO	FM	HOULIHAN J.M.	40 Hows Road,	Toombul	4012	266 3155
HO	OM	HUTCHINSON J.E.	2 Billabong Street,	Kenmore Hills.	4069	378 4694
Sn31/2	SM	HYNES R.	116 Ferndale Street,	Annerley	4103	848 6716
HO-USA	OM	INNES K.W.	32 Liatoki Street,	Mansfield.	4122	349 1013
HO-USA	OM	JENKINS D.L.	35 Alice Street,	Kingston	4114	209 4252
SN31/2	OM	JIRASEK J.	33 Florence Street,	Annerley.	4103	391 5537
SN31/2	FM	JOHNMAN N.G.	7 Irving Court,	Collingwood Park	4300	
SN31/2	FM	JOHNMAN Mrs J.A.	7 Irving Court,	Collingwood Park	4300	
HO	OM	KALLE K.	4/4 Chelmsford Avenue,	Lutwyche.	4030	57 9497
HO/HOn31/2	OM	KOPITKE E.	98 Yallabee Road,	Jindalee.	4074	376 4339
TT	OM	LYTHALL G.	340 Bridge Street,	Toowoomba.	4350	07634 5815
HON31/2	OM	MACAULAY A.	26 Damper Drive,	Andergrove	4740	07955 2182
N	OM	MACDONALD A.W.	14 Brennan Street,	Stafford	4053	352 5760
O	OM	MAHONEY P.M.	9 Soden Street,	Greenslopes	4120	397 2199
OO	OM	MAKIN J.	34 Marian Street,	Miles.	4415	07427 1480
Sn31/2QR	OM	MALONE S.	2/38 Grace Street,	Toombul	4012	
HO	FM	MALONE Mrs D.A.	2/38 Grace Street,	Toombul	4012	
O	SM	MALONE C.A.	27 Toombul Terrace,	Nundah	4012	
	OM	MANN N.A.	1 Whitwood Road,	Ebbyvale	4304	282 2033
HO-USA	OM	MAWSON R.	633 Webster Road,	West Chermide	4032	07359 1031
HO	FM	MAWSON Mrs C.	633 Webster Road,	West Chermide	4032	" "
HO/N	OM	McCONNELL F.B.	17 Stapylton Street,	Coolangatta	4225	07536 2807
O	OM	McTAGGART C.H.	101 Laura Street,	Ekibin	4121	892 1484
HO	FM	MEWES D.J.	97 Silkwood Street,	Algester.	4115	273 2014
HO	FM	MEWES Mrs P.A.	97 Silkwood Street,	Algester	4115	
N/OD/HO	OM	MILNE H.J.O.	19 Talbot Street,	Coorparoo.	4151	397 9603
HO	OM	MOORE S.K.	279 Cliveden Avenue,	Oxley	4075	379 8317
HO	OM	MORGAN A.	Lot 4 Naomi Road,MSF 454,	Lowood	4311	
HO	OM	MORLAND D.	186 Tallebudgera Drive ,	Palm Beach	4221	07535 8459
N	SM	MORLEY B.J.	144 Fort Road,	Oxley	4075	375 5463
N	FM	MORLEY Bob.	144 Fort Road,	Oxley	4075	
Sn31/2	FM	NEWELL J.D.	75 Colonsay Street,	Middle Park	4074	07376 5321
Sn31/2	FM	NEWELL A.J.	75 Colonsay Street,	Middle Park	4074	
Sn31/2	FM	NEWELL I.D.	75 Colonsay Street,	Middle Park	4074	
HO/OD	FM	NIXON K.	14 Archie Street,	Nambour.	4560	
HO/OD	FM	NIXON Mrs D.	14 Archie Street,	Nambour.	4560	
HO	FM	PARKES H.L.	33 Kalinga Street,	Clayfield	4011	357 6373
	FM	PARKES R. Mrs	33 Kalinga Street,	Clayfield	4011	
HO	OM	PETERSEN B.K.	4 Dargo Place,	Algester	4115	273 2164
TTN31/2	OM	PERKINS G.N.	7 Lisa Court,	Goodna.	4300	288 4295
HO n3	SM	POWELL W.	P.O. Box 6009,Cairns Mail Ctr.	Cairns	4871	07053 6041
HO	OM	ROBINSON A.C.	142 Northgate Road,	Northgate.	4013	266 4486
HO/HOn31/2	FM	ROBINSON G.A.	142 Northgate Road,	Northgate	4013	
Sn31/2	OM	SCRIVENS P.A.	1 Ronald Street,	Bray Park	4500	
HO	OM	SEARL H.D.	807 Upper Brookfield Road,	Upper Brookfield.	4069	374 1752
N-QLD	OM	SHARP D.	P.O. Box 220,	Cannon Hill.	4170	395 6404
S-UK &AUST	OM	SIVITER F.	Cottonvale,	via, Warwick.	4375	07685 2217
HO	SM	SMERDON K.G.	4 Bourke Street,	Innisfall	4860	
	FM	SPARGO R.K.	857 Samford Road,	Keperra	4054	355 4801
	FM	SPARGO A.	857 Samford Road,	Keperra	4054	
	FM	SPARGO K.	857 Samford Road,	Keperra	4054	
HO	SM	STANDFAST D.R.	17 Chapel Street,	Banyo	4014	267 5366
HO/HOn21/2	FM	STRAUTMANIS P.E.	41 Jessie Street,	Petrie Place	4000	368 1140
HO	FM	STRAUTMANIS K. Mrs	41 Jessie Street,	Petrie Place	4000	
HO	OM	STURMEY P.	29 Duncombe Road,	Narangba	4504	204 2304
	FM	STURMEY J.	29 Duncombe Road,	Narangba	4504	
	FM	STURMEY A.	29 Duncombe Road,	Narangba	4504	
	FM	STURMEY M.	29 Duncombe Road,	Narangba	4504	
HO/HOn21/2	PLM	SUGGIT S.	6 Medika Street,	Anglers Paradise.	4216	
HO	OM	THOMAS K.J.	MS544,"Ravensthorpe",	Clifton.	4361	
OO	OM	THORNTON D.J.	11 Dangerie Street,	Kallangur.	4503	204 6321
HO	OM	TREACY J.	34 Cambridge Street,	Rockhampton	4700	



HO/HOn31/2	PLM	WALL C.W.	8 McEwan Street,	Riverview.	4303	282 3538
N	OM	WALL S.J.	8 MacEwan Street,	Riverview.	4303	282 3538
HO/HOn31/2	FM	WARD E.W.H.	13 Loveanus Street,	Silkstone	4304	281 9518
HO	FM	WARN D.E.	10 Leach Avenue,	Landsborough.	4550	
HO	FM	WARN Mrs L.M.	10 Leach Avenue,	Landsborough	4550	07194 1510
SN31/2&HON	SM	WEBER T.R.	20 Rosemary Street,	Bellbird Park	4300	288 1780
N	OM	WETZIG B.J.	260 Kitchener Road,	Stafford Hgts	4053	359 0337
HO	OM	WETZIG J.H.	16 Pangeza Street,	Stafford	4053	359 3072
HO/HOn3	OM	WHELAN G.S.	22 Sunbird Avenue,	Paradise Point	4216	57 1677
HO/HOn31/2	OM	WOODS L.W.	P.O. Box 34,	Red Hill.	4059	369 5385
Sn3.5	OM	WRIGHT G.	20 Rhodes Street,	Loganlea.	4204	200 8807
HO/N-USA	OM	YUILLE J.D.	14 Moonarie Street,	Sunnybank Hills	4109	
AUSTRALIAN MODEL RAILWAY ASSOCIATION						
WEST AUSTRALIAN BRANCH- MEMBERSHIP LISTING 1 NOVEMBER, 1986.						
HO	GM	A.R.M.A.-W.A. BRANCH	P.O. Box 60,	Maylands	6051	
OO	OM	ADAMS R.M.	P.O. Box 70,	Scarborough	6019	341 8781
HO	OM	AMEY P.	19 Jessie Road,	Gooseberry Hill	6076	293 2023
OO(1939)	OM	ASHE R.W.	50 Canning Avenue,	Mount Pleasant	6153	
OO	OM	BAKKER P.	7 Marloo Road,	Greenmount	6056	294 2776
OO	OM	BLINCO T.R.K.	141 Mickleham Road,	Morley	6062	279 3765
OO	SM	BLINCO N.	141 Mickleham Road,	Morley	6062	
HO	OM	BOVEY L.T.	289 Cape Street,	Yokine	6060	349 6863
HO	OM	BRADBROOK M.N.	4 Murray Drive,	High Wycombe	6057	454 7553
N/Z	FM	BRADNEY J.	19 Lawley Road,	Lesmurdie	6076	291 8562
N/HO/O	OM	BRADNEY K.D.	19 Lawley Road,	Lesmurdie	6076	291 8562
N/Z	FM	BRADNEY P.C.	19 Lawley Road,	Lesmurdie	6076	
HO/OO	OM	BRISTOW G.P.	27 Dorchester Avenue,	Warwick	6024	448 4491
N	OM	BRIGHT C.D.	41 State Street,	Victoria Park	6100	361 5195
OO/HO	FM	BROWN G.A.	183 Kenwick Road,	Kenwick	6107	459 6944
	FM	BROWN N.	183 Kenwick Road,	Kenwick	6107	459 6944
OO/HO	FM	BROWN Mrs W.A.	183 Kenwick Road,	Kenwick	6107	
O	OM	CANTWELL W.R.	27 Driscoll Way,	Morley	6062	
OO	OM	CHAPMAN B.J.	86 Ivanhoe Street,	Eden Hill	6054	
HO	OM	CHAPPLE N.L.	21 Wicks Street,	Morley	6062	378 1397
HO	OM	COATES D.E.	45 Charles Road,	Armadale	6112	399 2919
OO	OM	COCKER C.	6 Snell Street,	Maylands	6051	271 8683
HO/OO	OM	COLCHESTER E.J.	10 Grinstead Way,	Balga	6061	344 2391
HO-USA	OM	CONGDON K.	2 Burnett Street,	Embleton	6062	271 6945
HO-SNCF	OM	CONGDON R.	2 Burnett Street,	Embleton	6062	
HO	OM	DE GROOT C.	20 Millport Drive,	Warwick	6024	447 1939
OO	OM	DEAN P.N.	63 Orrong Road,	Lathlain	6100	361 1182
	OM	DERRICK K.J.	4 Harold Road,	Maida Vale	6057	454 7554
HO/OO	OM	DESMALAN A.R.	P.O. Box 374,	Bentley	6102	361 9413
HO/Sn31/2	OM	DONKIN T.F.	2 Nanba Street,	Newman	6753	09175 1465
HO/Sn31/2	SM	DONKIN B.	2 Nanba Street,	Newman	6753	
HO	OM	DUNN R.A.	323 Vampire Avenue,	Paraburdoo	6754	
HO	OM	DURHAM A.T.	60 Kenny Street,	Bassendean	6054	279 6765
HO	FM	EAGLES J.	26 Swan Road,	High Wycombe	6057	
HO	FM	EAGLES Mrs M.	26 Swan Road,	High Wycombe	6057	
OO	OM	EDGEELL D.J.	140 Fulham Street,	Kewdale	6105	277 6832
N	OM	EDWARDS D.	8 Glyde Road,	Lesmurdie	6076	291 7002
HO/Sn21/2	OM	EDWARDS R.L.	3 Kiama Road,	Armadale	6112	
OO	OM	ELKINGTON C.A.	26 Dryandra Crescent,	Greenmount	6056	
N	OM	ELLIOTT C.F.	1 Bruton Street,	Balcatta	6021	349 6510
OO	OM	ENGLUND E.A.	5 William Street,	Shenton Park	6008	
OO	OM	FALCONER J.V.	22 Holdhurst Way,	Morley	6062	276 7710
N	FM	FEATHERSTON M.	14 Saw Drive,	Darlington	6070	299 7000
N	FM	FEATHERSTON R.	14 Saw Drive,	Darlington	6070	
TT/N31/2	OM	FRAYNE R.C.	4 Robinson Street,	Inglewood	6052	271 4708
OO	OM	FRYER R.	1 Hurley Way,	Hillarys	6025	401 3514
HO/HOn21/2	OM	GIBBONS A.J.	141 Hayes Avenue,	Dianella	6062	349 3942
OO	OM	GLADWIN F.	7 Damerham Road,	Armadale	6112	399 2851
OO	OM	GRAY A.H.	258 Grove Road,	Lesmurdie	6076	097291 8050
1	OM	GRIEVE I.	5 Vigna Place,	Ferndale	6155	
OO	OM	HAMILTON A.R.	2a Bridge Street,	Collie	6225	09734 3997
EM	OM	HANCOCKS G.H.	27 Schmitt Road,	Kalamunda	6076	293 2276
OO	OM	HANDS R.E.	8 Bath Road,	Embleton	6062	276 9638
	FM	HASKELL D.	12 Hartley Court,	Greenwood	6024	
	FM	HASKELL K.	12 Hartley Court,	Greenwood	6024	
SN31/2	OM	HAYTER L.A.	166 Great Eastern Highway,	South Guildford	6055	279 6069
OO "BR"	FM	HIDDEN J.R.	3 Chiltgrove Way,	Balga	6061	344 3713
OO "BR"	FM	HIDDEN J.	3 Chiltgrove Way,	Balga	6061	
HO	OM	HODGES B.	6 Uringa Way,	Wanneroo	6065	405 3495
HO/N	OM	HODGSON D.	280 Benara Road,	Morley	6062	279 9912
HO	OM	HOOD R.D.	8/6 York Street,	Inglewood	6052	271 3727
OO	OM	HORTON G.S.	29 Armour Way,	Lesmurdie	6076	291 8169
HO	OM	HUG S.J.	P.O. Box 1492,	Geraldton	6530	09921 2160
N	OM	HUGHES L.H.	229 Balcatta Road,	Balcatta	6021	344 3547
OO/HO	OM	JUMEAUX A.N.C.	8 Ager Street,	Dianella	6062	275 1895
OO/HO	FM	KEENS B.G.	28 Lovegrove Way,	Morley	6062	276 1648
OO/HO	FM	KEENS S.	28 Lovegrove Way,	Morley	6062	
7mm	OM	KEHOE P.	49 Orbell Way,	Bullcreek	6155	332 3879
HO	OM	KELLY G.D.	38 Edney Road,	High Wycombe	6057	454 5873



4mm	OM	KERR T.	42 Shearwater Drive,	Stirling	6021	
HO	OM	KROYER N.J.	7 Denic Rise,	Leeming	6155	332 6275
HO	OM	LAMB G.O.	P.O. Box 333,	Karratha	6714	04185 1298
SN31/2	FM	LEWIS C.A.	48 Harold Street,	Dianella	6062	276 7914
SN31/2	FM	LEWIS Mrs E.	48 Harold Street,	Dianella	6062	
OO/SN31/2	OM	LING D.A.	245 Anzac Terrace,	Bayswater	6053	378 2509
HO/DO	OM	LORIMER D.B.	8 Fluellen Way,	Hamilton Hill	6163	418 3979
N(DB)	OM	LUDLOW J.R.	19 Hewitt Way,	Booragoon	6154	" "
HO	OM	MAJER J.D.	Sch of Biology, W.A INST.OF TECH	Bentley	6102	
OO	OM	MALTON D.G.	4 Gardiner Street,	Roelands	6226	09726 3124
N	OM	MANGAN R.F.	C/- Post Office,	Koolan Island	6733	09191 1575
OO/LNER	OM	MARTIN J.V.	3 Albion Place,	Carine	6020	447 5490
OO/HO	OM	MCGUINNESS B.J.	14 Wheeler Street,	Morley	6062	276 7037
HO	OM	MEAD J.	68 Alexander Street,	Wembley	6014	387 1128
HOn3	OM	MEAD S.J.	11 Grant Street,	Embleton	6062	276 8745
HO/DO	OM	MILLARD K.	8/25 Cape Street,	Osborne Park	6017	444 2995
N	OM	MILLER J.D.	35 Brandon Street,	South Perth	6151	367 9308
OO/O	OM	NEVILLE J.H.	200 Grove Road,	Lesmurdie	6076	291 8842
OO	OM	NEWMAN K.	9 Marlock Road,	Woodlands	6018	446 9992
EM	OM	NOBLE G.	79 Gavour Road,	Wattle Grove	6107	453 6858
HO(USA)	OM	ODGERS B.L.H.	253A Welshpool Road,	Queens Park	6107	350 5694
ON3	OM	O'SHEA K.J.	Townhouse H48 Shakespeare St	Karratha	6714	09185 4725
OO/DO-9	OM	PAGRAM J.E.	136 Angela Street,	South Perth	6151	367 3940
HO	OM	PATERSON C.G.	13 Kambalda Way,	Kewdale	6105	277 5904
HO/DO	OM	PILMOOR G.T.	16 Cockram Place,	Beechboro	6063	279 1689
N	FM	POROCK A.	27 Dolomite Road,	Carine	6020	448 8038
N	FM	POROCK D.	27 Dolomite Road,	Carine	6020	448 8038
OO	OM	PORTER A.A.	21 Prosser Way,	Myaree	6154	330 1848
OO	OM	PUGH S.H.	6 Myrtle Street,	Perth	6000	328 9161
N	OM	RAYNER S.H.	P.O. Box 200,	Halls Creek	6770	09168 6187
HO	OM	REID A.G.	57 Gladstone Road,	Rivervale	6103	362 2452
HO	OM	RYAN J.F.	34 Simper Street,	Wembley	6014	387 2195
HO	OM	SCARFE P.N.	526 Hunt Way,	Karratha	6714	09185 2610
HO/DO	OM	SCHWAGERMANN K.(JNR.)	9 Ramsdale Street,	Scarborough	6019	
HO	OM	SHENTON T.C.	43 Parkside Avenue,	Mt. Pleasant	6153	364 6830
OO	OM	SIBLEY M.R.	26 Marri Road,	Duncraig	6023	447 8126
OO	OM	SMART R.T.	9 Henly Road,	Mount Pleasant	6153	364 2763
OO	OM	SNOOK B.G.	11 Kambalda Way,	Kewdale	6105	277 4485
SN31/2	OM	STALLARD G.J.	14/6 Coolgardie Avenue,	East Fremantle	6158	339 6361
Sn 3.5	FM	STALLARD R.	35 Geddes Street,	Victoria Park	6100	362 4340
Sn 3.5	FM	STALLARD Mrs. J.	35 Geddes Street,	Victoria Park	6100	
HO/DO	SM	STANTON C.	4 Clark Road,	Morley	6062	276 9824
OO	OM	STOKES D.T.	40 Napier Road,	Morley	6062	275 4508
OO	OM	STREET M.A.	27 Leander Street,	Beldon	6025	401 9422
OO/EM	OM	TAYLOR C.J.	57 Upton Street,	St James	6102	
OO	SM	TAYLOR K.J.	16 Stanley Street,	Nedlands	6009	386 7346
OO	OM	TEMPERLEY B.A.	2 Garton Place,	Duncraig	6023	448 3391
EM	OM	THODAY B.E.	30 Abercain Way,	Parkwood	6155	457 4256
SN31/2	OM	THOMPSON M.E.	38 Buntine Road,	Wembley Downs	6019	446 8624
SN31/2	OM	VERGA P.	70 Cawston Road,	Attadale	6156	330 6061
N	OM	VOSPAP H.C.W.	75 Lilburne Road,	Duncraig	6023	448 6525
SN31/2	OM	WATSON G.R.	30 Wynne Street,	Hazelmere	6055	
HO	OM	WATTS F.S. Jnr.	C/- 29 Tooting Street,	Beckenham	6107	458 5129
OO9	OM	WATTS J.	2 Maritime Avenue,	Kardinya	6163	337 7296
HO	OM	WHITE I.L.	7 Creaney Drive,	Kingsley	6026	
On2	OM	WHITE J.	40 Carcoola Street,	Nollamara	6061	349 3553
EM(G.W.R.)	OM	WORSNOP P.C.	109 First Avenue,	Bassendean	6054	279 7362
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National Model Railroad Assoc.	Incorporated.	4121 Cromwell Road, Chatt TN37421, U.S.A.
New Zealand Association of	Model Railway Clubs Inc.,	Box 26073, P.O., Newlands,
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U.S.S.R. National Public Library	of Science and Technology,	12 Kuznetskii Most, Mosco U.S.S.R.
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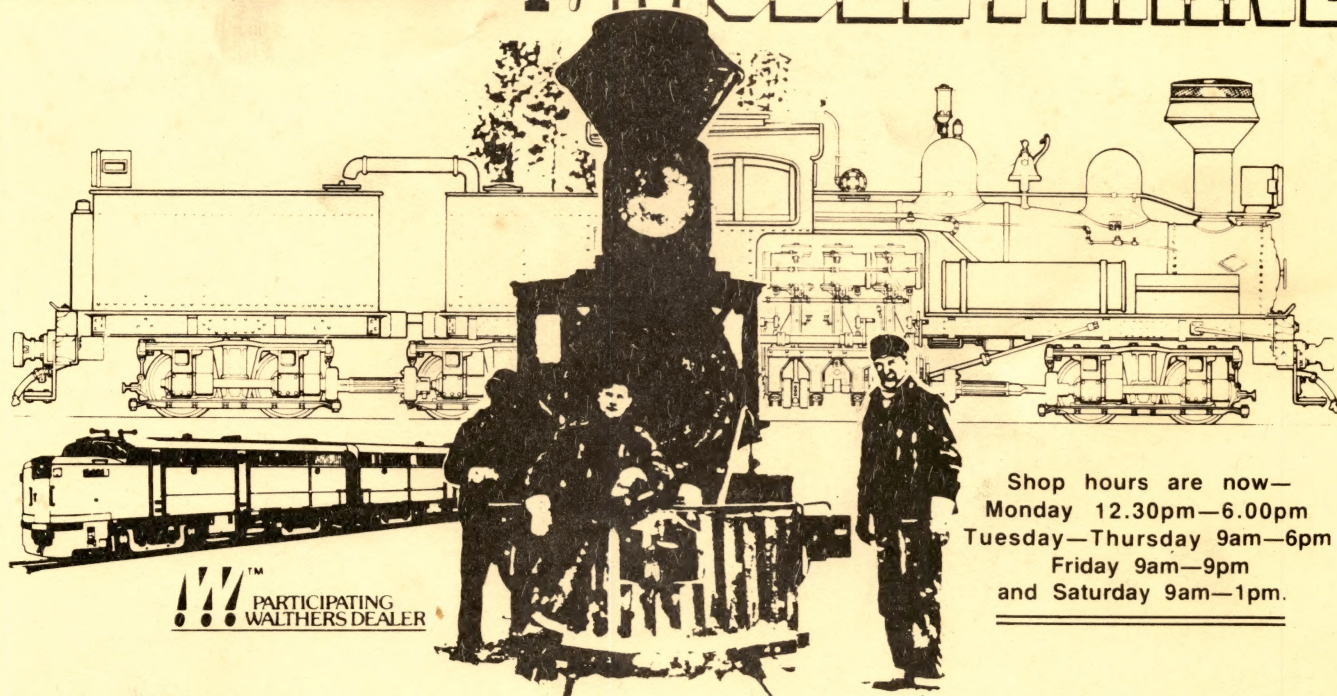
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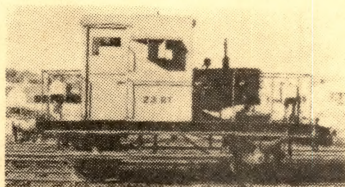
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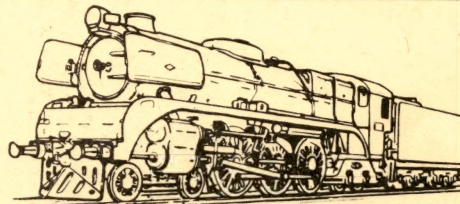
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